

STRATEGIC PLANNING

DRAFT **Randwick** **Development** **Control Plan** **D1 Kensington** **and Kingsford** **Town Centres**

D05384635

18 November 2025



File reference	D04579626	Date	Signature
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Part A

1. Introduction

1.1. Kensington and Kingsford town centre planning review

The Kensington and Kingsford town centres located along Anzac Parade, comprise an important urban renewal corridor in the Randwick City local government area. The town centres have been subject to a comprehensive planning review undertaken over 2016-2019 to address projected population growth and expected demographic changes, improve the quality of building design and the public realm, and accommodate the Sydney City to South East Light Rail infrastructure along Anzac Parade.

The culminating Planning Strategy: Kensington and Kingsford town centres (“The Strategy”) contains a suite of key directions, objectives, strategies and actions to guide the future sustainable growth and development of the town centres. The objectives and controls in this Part of the DCP are based on the extensive site and built form analysis and modelling informing the Strategy for these centres.

1.2. Alignment with other planning instruments

This section applies to all new development and alterations and additions to existing development on land situated within the Kensington and Kingsford town centres (Figure 1). The controls supplement the provisions of the Randwick Local Environmental Plan 2012 (RLEP 2012) and aim to deliver high quality building and urban design and to promote a high level of liveability and economic and employment opportunities in the Kensington and Kingsford town centres.

In addition to the RLEP 2012, a number of State Environmental Planning Policies (SEPPs) apply to certain types of development within the town centres, depending on the nature of the proposal. The key ones are:

- State Environmental Planning Policy (Housing) 2021 and supplementary Apartment Design Guide (ADG)
- State Environmental Planning Policy (Transport and Infrastructure) 2021 (T&ISEPP)
- State Environmental Planning Policy (Industry and Employment) 2021

In the event of an inconsistency between this DCP and a relevant SEPP, the SEPP prevails to the extent of the inconsistency.

This Part of the DCP should be read in conjunction with:

- Part A - Introduction
- Part B - General Controls
- Part C - Land use Controls
- Other Parts of the DCP for specific development types, sites or locations, if relevant to the application.

Figure 1a: The lands to which this DCP applies- Kensington Town Centre

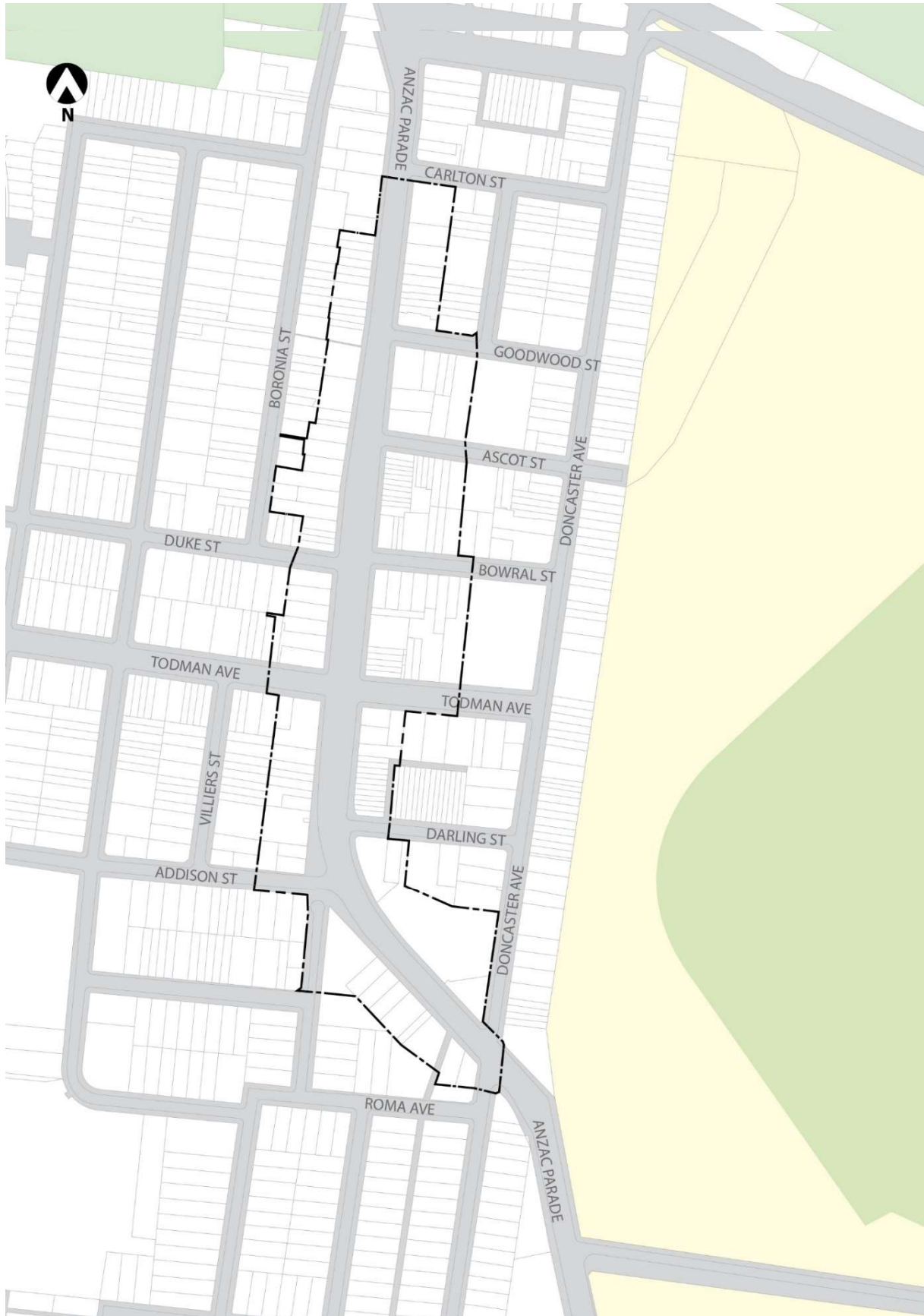


Figure 1b: The lands to which this DCP applies- Kingsford Town Centre



2. Urban design and place-making

2.1. Guiding principles

Development within the Kensington and Kingsford town centres must align with the following urban design and place making principles which are derived from the K2K Planning Strategy and community input:

- Provide quality affordable housing to meet local housing needs, particularly for key workers, essential workers and students
- Reinforce boulevard character along Anzac Parade by strengthening the built form edge and adding greenery
- Achieve a dominant typology of diverse mid-rise, mixed-use buildings throughout the town centres
- Provide taller, slender landmark buildings at identified strategic node sites in conjunction with the delivery of substantial public benefits established through a design excellence process
- Protect the heritage significance of heritage items, contributory buildings and/or heritage conservation areas located within the town centres and adjoining areas
- Give priority to people walking, cycling and using public transport
- Achieve a sensitive transition in relation to recently constructed development and surrounding established lower scaled residential neighbourhoods
- Create a positive street level environment through built form that allows solar amenity, permeability and maintains human scale
- Ensure that new infill development respects the fine-grain character of contributory buildings
- Establish building setback controls which provide for the creation of wider footpaths and street tree planting
- Achieve urban design, place and architectural excellence, including best practice environmental design
- Provide active street frontages throughout the town centres
- Encourage precinct-scale benefits across all node sites that contributes to the unique character of each town centre; and
- Achieve innovative place-led solutions for local hydrology and resilience.

Controls

- a) A statement prepared by a suitably qualified architect or urban designer must be submitted with all DAs to demonstrate consistency with the Guiding Principles of this Part.

3. Desired future character

3.1. Kensington and Kingsford town centres

The following desired future character statements for the Kensington and Kingsford town centres establish the directions and key outcomes required to be achieved through development within each precinct. Amendments to the Randwick Local Environmental Plan 2012 (RLEP 2012) will provide the foundations to ensure sustainable growth in housing and employment and public benefits for both Kensington and Kingsford town centres.

Kensington Town Centre: statement of desired future character

Kensington is to evolve into a vibrant and dynamic town centre situated along Anzac Parade, Sydney's finest grand green boulevard. The town centre will be well connected and highly accessible, capitalising on its proximity to key employment hubs including the Randwick Health and Education Super Precinct and the Sydney CBD. Kensington town centre will offer an exciting city apartment lifestyle, with buildings designed to the highest quality and offering excellent amenity to residents. A range of housing types including affordable housing will be woven into the town centre's urban fabric to offer housing diversity and choice to a wide range of people including the elderly, students and families. The integrity of existing heritage and contributory buildings located within and adjoining the town centres will be respected and integrated with high quality and sympathetic contemporary architecture that enhances the character and layering of the urban experience.

Kensington town centre will be a focus for creativity and innovation. A gallery/creative space at Todman Square will create a cultural anchor for the town centre, supported by a diverse range of cafes, restaurants and shopping options that attract visitors from across Sydney. Economic and social transformation of the centre will be facilitated by creating a cohesive and attractive streetscape with reduced visual clutter. Innovative status will translate cutting edge research into real world business success. The town centre will have a green identity, setting the bench mark for sustainability within the Local Government Area (LGA) through Ecologically Sustainable Development (ESD) targets, Water Sensitive Urban Design (WSUD) practices, high quality green public places with linkages to nearby parks, and sustainable transport modes such as the light rail, buses, cycling and walking.

Kingsford town centre: statement of desired future character

Kingsford will develop into an exciting and dynamic town centre continuing to draw on its rich multi-cultural identity. The town centre will provide a diverse offer of restaurants, cafes and retail shopping, set within a rejuvenated public domain that supports activation and social interaction. The town centre will be a safe and inclusive place to live, work and visit. Buildings will be designed to the highest quality incorporating a mix of apartments, laneway mews and affordable housing. Highly connected and accessible, the town centre will foster hubs of activity focused around the terminus at Kingsford Junction and Kingsford Mid-Town, the old heart of Kingsford.

Economic and social transformation of the centre will be facilitated by creating a cohesive and attractive streetscape with reduced visual clutter. The town centre will have a green focus, and set a new performance benchmark for sustainability within the LGA through ESD targets, WSUD practices, public places with canopy trees and landscaping and support of sustainable transport modes such as the light rail, buses, cycling and walking. The integrity of existing heritage items, contributory buildings and/or heritage conservation areas within and around the town centres will continue to be respected and integrated, through high quality architectural design.

Innovative business start-ups will be encouraged to provide a 'bridge' between research and business.

3.2. Strategic node sites

The Strategy identifies strategic node sites adjacent to light rail infrastructure which can accommodate additional height and density provided a high standard of design excellence is demonstrated. These strategic node sites will help define the corners through taller landmark buildings and enable a hub of activity where more intensive business floor space and community spaces are achieved.

Part B establishes the desired future character, key directions and block by block development outcomes for the following strategic node sites (identified in Figures 2a and 2b) within the Kensington and Kingsford town centres:

- Todman Square Precinct
- Kingsford Midtown Precinct; and
- Kingsford Junction Precinct.

Controls

- a) Submit a statement with the DA prepared by a suitably qualified architect or urban designer demonstrating how the proposed design meets the desired future character of the relevant town centre, and where applicable, the strategic node site based on the block controls contained in Part B of this chapter of the DCP.

Figure 2a: Location of strategic node sites in Kingsford

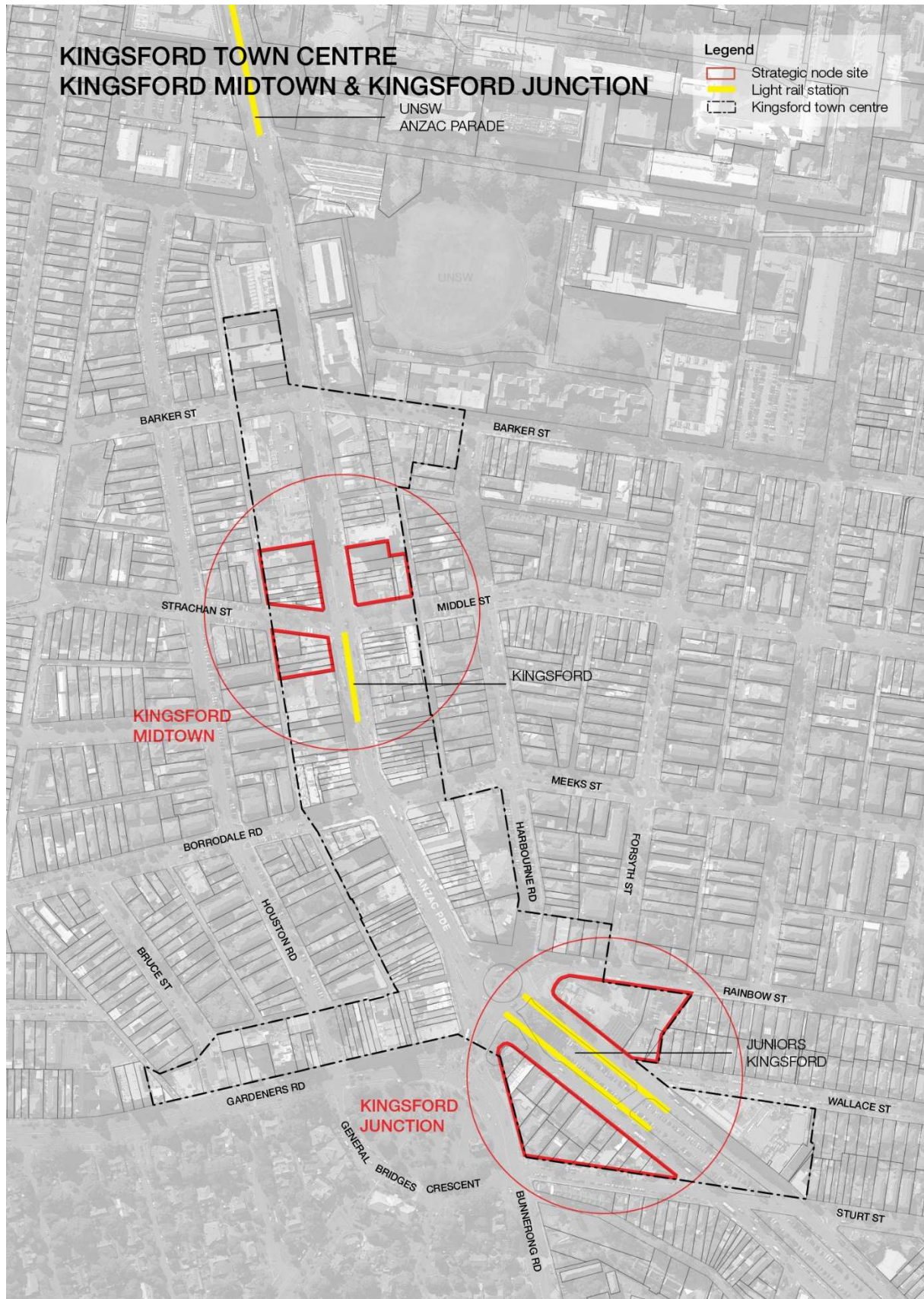


Figure 2b: Location of strategic node sites in Kensington



4. Design excellence

4.1. Explanation

‘Design excellence’ has been established as the driving urban design principle for future development of the Kensington and Kingsford town centres to foster an enriched experience of the urban environment.

The consideration of ‘design excellence’ is a requirement under RLEP 2012 (clause 6.11) for proposals involving buildings over 15m in height, or for sites that are over 10,000m² in size or for land where a site-specific development control plan is required. In addition, key strategic node sites (Kingsford Junction, Kingsford Midtown and Todman Square) are expected to achieve a performance benchmark in design innovation and sustainability beyond what is presently required, via a design excellence architectural competition process.

The RLEP 2012 specifies the applicable height and floor space ratio controls for sites across Kensington and Kingsford town centres. Bonus height and floor space ratio provisions under the RLEP 2012 may apply when the development exhibits design excellence as judged under an architectural design competition and the provision of social infrastructure.

Objectives

The objectives for design excellence are to:

1. Achieve outstanding architectural, urban and landscape design within the Kensington and Kingsford town centres
2. Deliver high quality landmark buildings that contribute positivity to their surroundings and help to create a sense of place and identity
3. Enhance the character, aesthetic quality, functionality, and amenity of the Kensington and Kingsford town centres
4. Encourage higher energy, water and waste performance ratings for residential development and
5. Facilitate the delivery of place-based social infrastructure.

Controls

- a) All new development involving the construction of a new building or external alterations to an existing building is to meet the requirements of Clause 6.11 of the RLEP 2012 relating to design excellence
- b) Buildings are to be designed to achieve at least 5 Star Green Star performance as a component for achieving design excellence on strategic node sites
- c) DAs involving the construction of a new building on the following strategic node sites are subject to an architectural design competition in accordance with Clause 6.21 of RLEP 2012:
 - Todman Square Precinct
 - Kingsford Midtown Precinct
 - Kingsford Junction Precinct
- d) Prior to lodgement of DAs for strategic node sites, the architectural design competition process is to be undertaken in accordance with Council’s “Architectural Competition Policy” adopted 10 December 2019
- e) For DAs at strategic node sites that successfully demonstrate design excellence, the consent authority may consider the following:

- i) additional building height and FSR in accordance with the RLEP 2012 Additional Heights and Additional FSR maps; and
- ii) exclusion of social infrastructure floor space provided on the site from the total gross floor area calculation, subject to the social infrastructure floor space being dedicated to Council.

Note

Refer to the Randwick City Architectural Competition Policy for further information on the requirements for holding an architectural design competition.

Note

A number of strategic node sites have been identified for the physical provision of social infrastructure as part of the design excellence competition process as follows:

- Todman Square Precinct: Multi-functional creative space, innovation centres and public art
- Kingsford Midtown Precinct: Innovation centre; and
- Kingsford Junction Precinct: Community hub

Refer to Part B- Block by block controls for further information.

5. Floor space ratio

5.1. Explanation

Clause 4.4 of the RLEP 2012 provides Floor Space Ratio standards for the Kensington and Kingsford town centres. The maximum FSR that can be achieved on a site is shown on the RLEP 2012 FSR Map. An alternative FSR is applicable in accordance with the RLEP 2012 Alternative FSR Map where the proponent makes an offer to enter into a Voluntary Planning Agreement for either a monetary contribution or the delivery of Community Infrastructure in accordance with the Community Infrastructure Contributions Plan (see Part D of this DCP).

In addition, a minimum non-residential FSR is applicable to strategic node sites at the Todman Square, Kingsford Midtown and Kingsford Junction Precincts to ensure there is sufficient floor space available to accommodate employment, retail and commercial services. A concentration of non-residential floor space at these strategic sites would foster vibrant nodes of activity near light rail stops, providing for a mix of shops, hospitality related businesses, creative and innovation spaces and other commercial services to serve the needs of residents, workers and visitors. It would also serve to support the economic development, employment growth and future investment and strengthen relationships within the wider Randwick Collaboration Area. Further information on the Randwick Collaboration Area and Place Strategy is available from the Greater Sydney Commission's website.

Objectives

The objectives for floor space ratio are to:

1. Support economic growth, job creation, art and cultural uses and innovation within the Kensington and Kingsford town centres
2. Enhance the day and night-time economies
3. Ensure sufficient floor space is available to support a mix of business, service, retail, creative, cultural, high-tech industry and recreational uses
4. Promote convenient access to shops, retail and services
5. Leverage commercial opportunities to support and align with the Randwick Collaboration Area Place Strategy, including delivery of innovation spaces, co-working hubs, multi-functional creative spaces and health and medical uses.

Controls

- a) The maximum FSR that can be achieved on a site is shown on the RLEP 2012 FSR Map. An alternative FSR is applicable in accordance with the RLEP 2012 Alternative FSR Map where the proponent makes an offer to enter a Voluntary Planning Agreement (VPA) for either a monetary contribution and/or the delivery of Community Infrastructure in accordance with the Community Infrastructure Contributions Plan (see Part D of this DCP for details on the Community Infrastructure Contribution)
- b) In relation to the Kensington Town centre where an existing FSR Map does not apply, the Alternative FSR Map is applicable for the purposes of calculating the Community Infrastructure Contribution referred to in clause a) for any floor space above the existing height maximum control shown on the RLEP 2012 Height Map
- c) A minimum non-residential FSR of 1:1 is to be provided at each strategic node site within the Todman Square, Kingsford Midtown and Kingsford Junction Precincts, in accordance with Clause 4.4 of the RLEP 2012

- d) Non-residential floor space must be designed to be accessible, useable and functional for the purposes of commercial, business, entertainment and retail activities and the like

Definition

Non-residential floor space ratio means the ratio of the gross floor area of that part of a building used or proposed to be used for any purpose in all buildings within a site to the site area, other than for any of the following purposes:

- Residential accommodation
- Tourist accommodation
- Student housing
- Parking areas, driveways and areas outside of buildings used for outdoor dining
- Roof top open space that is for the exclusive use of building occupants
- Telecommunications facilities.

6. Built form

6.1. Explanation

This section refers to the ‘three dimensional’ appearance of the Kensington and Kingsford town centres including the function, aesthetic quality, shape, scale and configuration of individual buildings, as well as their relationship to streets and the public domain. Controls focus on achieving an appropriate scale for new development so that buildings reinforce a coherent, harmonious and appealing urban environment, and contribute to the enhancement of the public realm. Refer to Part B block controls of this chapter which incorporate built form controls in this section into building envelopes.

Objectives

The objectives for the built form are to:

1. Ensure built form is compatible with the desired future character of each centre in terms of building bulk, scale and massing
2. Ensure coherent and orderly redevelopment of land and avoid isolation of sites
3. Ensure development reinforces the urban structure and street hierarchy
4. Ensure development responds to the existing siting, scale, form and character of heritage items, contributory buildings and adjoining properties
5. Ensure taller buildings are vertically proportioned with a slender form to minimise building bulk
6. Ensure street walls provide a human scale in the public realm
7. Achieve a scale transition between buildings within Kensington and Kingsford town centres and surrounding residential areas to protect residential amenity; and
8. Ensure that development does not unreasonably diminish sunlight and visual amenity to neighbouring properties and public spaces as well as communal spaces within the development site
9. Ensure that the number of storeys in a development aligns with the maximum permissible height in metres in the RLEP allowing for generous floor to floor heights, reasonable level of internal amenity, adequate height to accommodate structures such as plant rooms or lift overruns above the roof and flexibility for future changes of use
10. Allow adequate area between floors for the provision of services and noise attenuation
11. Provide upper level building setback controls to reinforce the desired scale of buildings, minimise overshadowing of the street and other buildings and create a cohesive streetscape environment.
12. Ensure that ground floor setback areas maintain visual openness, facilitate pedestrian movement, support landscaping, and preserve safety and access requirements.

Controls

Lot Amalgamation

- a) A minimum street frontage of 20m is to be provided for each development site along Anzac Parade and Gardeners Road
- b) When development/redevelopment/amalgamation is proposed, sites between and adjacent to developable properties are not to be limited in their future development potential
- c) Where a development proposal results in an isolated site, the applicant must demonstrate that negotiations between the owners of the lots have commenced prior

to the lodgement of the DA to avoid the creation of an isolated site. The following information is to be included with the DA:

- i. Evidence of written offer (s) made to the owner of the isolated site* and any responses received
 - ii. schematic diagrams demonstrating how the isolated site is capable of being redeveloped in accordance with relevant provisions of the RLEP 2012 and this DCP to achieve an appropriate urban form for the location, and an acceptable level of amenity
 - iii. schematic diagrams showing how the isolated site could potentially be integrated into the development site in the future in accordance with relevant provisions of the RLEP 2012 and this DCP to achieve a coherent built form outcome for the block.
- d) Where lot consolidation cannot be achieved to comply with the maximum envelopes in the block diagrams, alternative designs may be considered where the proposal exhibits design excellence and can demonstrate consistency with the relevant objectives of the block controls (Part B).

Note

A reasonable offer, for the purposes of determining the development application and addressing the planning implications of an isolated lot, is to be based on at least one recent independent valuation and may include other reasonable expenses likely to be incurred by the owner of the isolated property in the sale of the property. To assist in this assessment, applicants are to submit details and diagrams of development for the isolated site, that is of appropriate urban form and amenity. The diagram is to indicate height, setbacks and resultant footprint (both building and basement). This should be schematic but of sufficient detail to understand the relationship between the subject application and the isolated site and the likely impacts of the developments. Important considerations include solar access, deep soil landscaping, privacy impacts for any nearby residential development and the traffic impacts of separate driveways access.

The application may need to include a setback greater than the minimum requirement in the relevant planning controls. Or the development potential of both sites may need to be reduced.

Note

Development proposals that cannot achieve a minimum frontage of 20m are unlikely to realise the maximum FSR indicated for the site on the RLEP 2012 FSR maps given the application of the Apartment Design Guide and other DCP requirements. Applicants are advised to obtain professional design advice.

Building heights

- a) The maximum height that can be achieved on a site is shown on the RLEP 2012 Height Map. An alternative maximum height is applicable in accordance with the RLEP 2012 Alternative Height Map where the proponent makes an offer to enter into a Voluntary Planning Agreement (VPA) for either a monetary contribution or the delivery of Community Infrastructure in accordance with the Community Infrastructure Contributions Plan. (see Part D for details on Community Infrastructure Contribution)
- b) The maximum number of storeys on a site is to comply with the following:

- i. on sites with a maximum of 16m – 4 storeys
- ii. on sites with a maximum of 19m – 5 storeys
- iii. on sites with a maximum of 31m – 9 storeys
- iv. on sites with a maximum 57m – 17 storeys
- v. on sites with a maximum 60m – 18 storeys

Tower forms (strategic node sites)

- a) Buildings on strategic node sites are to be designed as slender towers, with a maximum floorplate of 600m² GFA for the tower form above the podium. A 10% variation on the maximum floor plate may be considered only where the proposal meets design excellence
- b) Towers are to be designed with vertical articulation demonstrating design excellence to create defining landmarks for the centres and incorporating a variety of techniques to create visual interest, high quality materials and finishes
- c) Tower forms must be well separated from other tower buildings and provide occupants with excellent amenity such as privacy, daylight, outlook and privacy
- d) Roof top mechanical equipment shall be sized and located so that it cannot be viewed from the public domain and adjoining buildings
- e) Architectural lighting should be designed to enhance key elements of the tower.

Definition

Under RLEP 2012, the **‘maximum building height’** is defined as: The vertical distance between resultant ground floor height and the highest point of the building, including plant a lift overruns, but excluding communication devices, antennae, satellite dishes, masts, flagpoles, chimneys, flues and the like.

Note

Plant lift and overrun and staircase on the roof level are not to be counted as a storey

Street walls

- a) Buildings must be designed with a street wall height of 4 storeys
- b) On sites with contributory buildings, the consent authority may consider a variation to the four-storey street wall height requirement of between 2 and 6 storeys if the design:
 - i. Results in an improvement to the contributory building in accordance with established heritage principles to avoid facadism
 - ii. Meets the objectives of this clause and exhibits design excellence
 - iii. Retains contributory or heritage elements; and
 - iv. Provides a transition to neighbouring sites.

Note

See Section 9 of this DCP for further requirements for heritage items and contributory buildings.

Note

Street wall height can be established via podiums, datum lines or other design elements.

Building Setbacks

- a) DAs are to comply with the minimum ground floor and upper level setbacks illustrated in the relevant block diagrams in Part B of this DCP
- b) Development that results in an exposed party wall on an adjoining building is to incorporate architectural or vertical landscape treatment to improve visual amenity
- c) The ground floor setback area along Anzac Parade and side streets is to be devoid of any permanent or temporary structures including but not limited to, built form, blade walls, raised planter boxes and terraces.

Note

See Part A Section 9 of this chapter for further requirements for heritage items and contributory buildings.

Building depth

- a) The residential component of development fronting Anzac Parade and Gardeners Road is to have a maximum building depth of 22m including balconies.

Definition

Building depth refers to the dimension measured from the front to the back of a building's floorplate. It has a significant influence on building circulation and configuration and impacts upon internal residential amenity such as access to light and air. For residential development, narrower building depths generally have a greater potential to achieve optimal natural ventilation and solar access than deeper floor plates.

Figure 3a Kensington building street setbacks

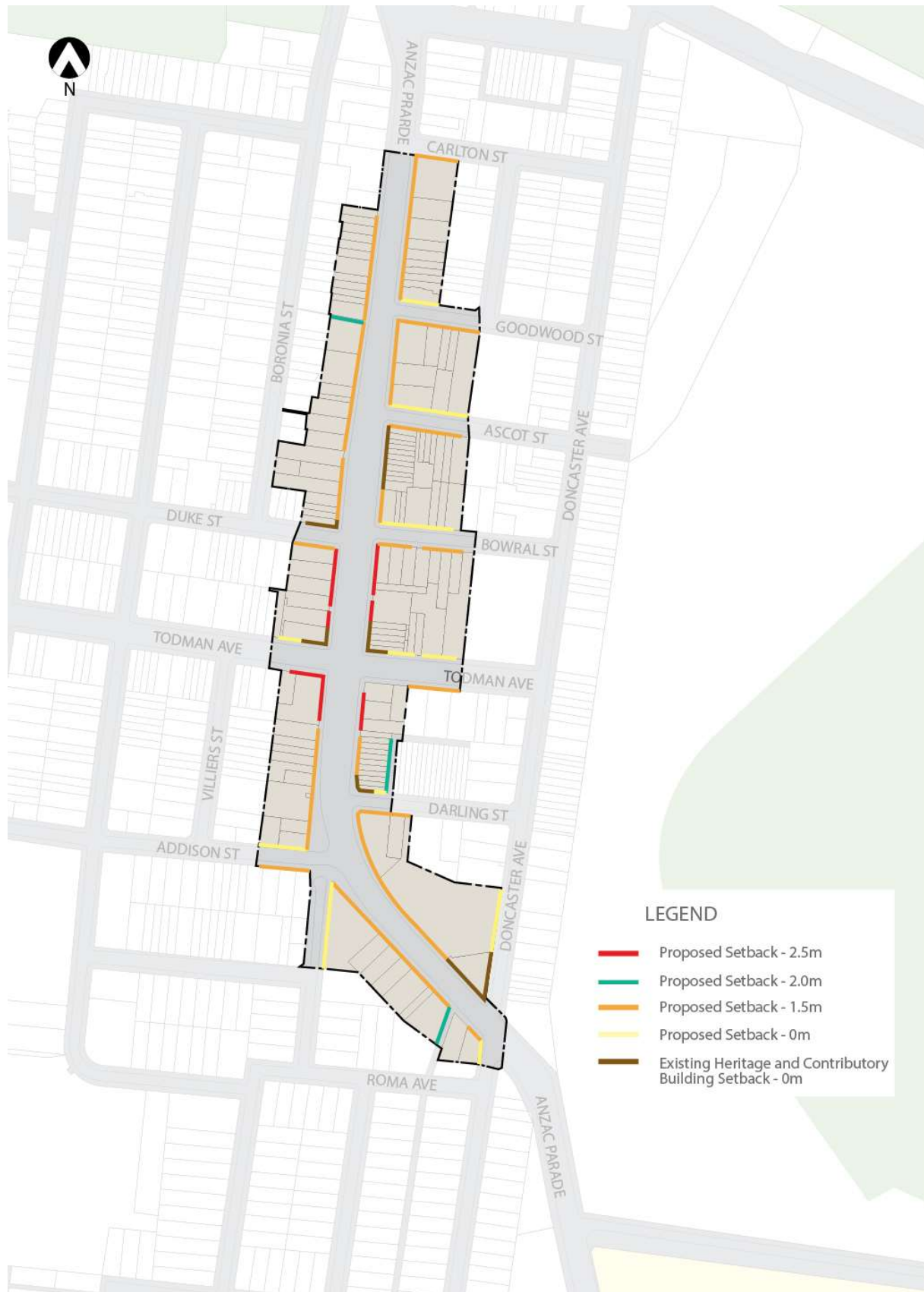
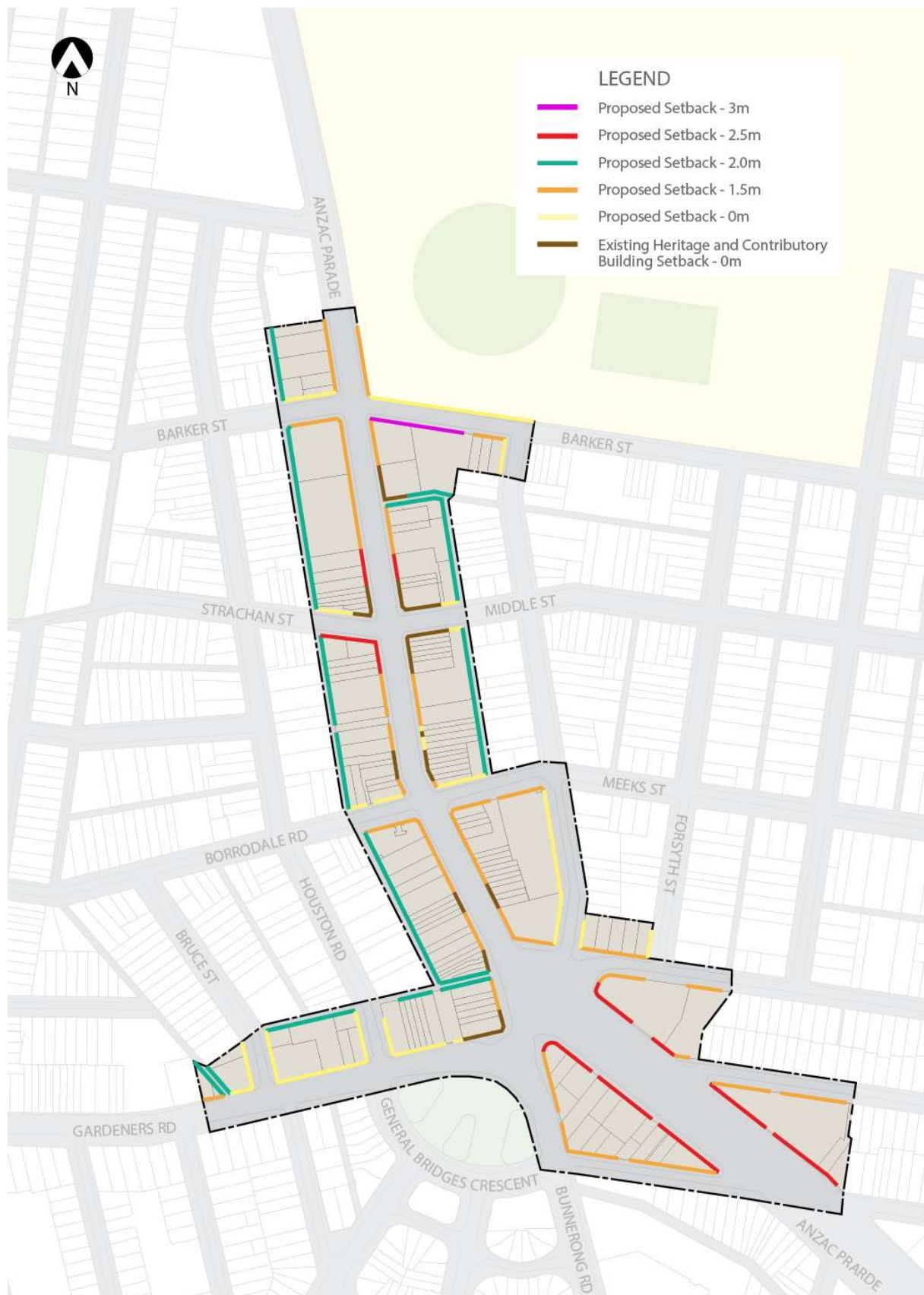


Figure 3b Kingsford building street setbacks



7. Through site links/mid-block connections

7.1. Explanation

Through-site/mid-block links provide key access routes for pedestrians and should be established in larger key sites and in those parts of the town centres where site consolidation is likely. Enhancing the town centres' pedestrian experience would improve walkability, provide increased opportunities for interaction and connection and contribute to passive surveillance.

Objectives

The objectives for site links are to:

1. Improve permeability and provide connections to main pedestrian and cycling networks, key destinations and residential areas
2. Ensure that the design of through site links and mid-block connections is high quality, safe, well-lit, accessible and pedestrian friendly
3. Encourage walking and cycling as part of the broader street network to promote community interaction, better health outcomes and reduce vehicle movements.

Controls

- a) Through site links and mid-block connections are to be provided in accordance with the relevant block diagram in Part B of this chapter
- b) Where new site links or variations are proposed, the consent authority is to consider the need for and desirability of the links or connections having regard to the objectives of this section
- c) Through site links and mid-block connections are to have an easement for public access on title or covenant on title unless identified for dedication to Council
- d) Through-site links/ mid-block links are to be designed to:
 - i. have a minimum width of 6m, and a clear height of at least 6m
 - ii. be direct and publicly accessible 24 hours a day
 - iii. allow visibility along the length of the link and be open to the sky as much as is practicable
 - iv. be easily identified by users and have a public character
 - v. include signage advising of the publicly accessible status of the link and the places to which it connects
 - vi. be clearly distinguished from vehicle accessways
 - vii. align with breaks between buildings so that views are extended and there is less sense of enclosure
 - viii. provide active edges and opportunities for natural surveillance
 - ix. include materials and finishes (paving materials, tree planting, furniture etc.) integrated with adjoining streets and public spaces and be graffiti and vandalism resistant
 - x. ensure no structures (for example, electricity substations, carpark exhaust vents, swimming pools etc) are constructed in the through-site link
 - xi. include landscaping to assist in guiding people along the link while enabling long sightlines.
- e) Through-site links are only to pass through or under a building where:

- i. the building's height is greater than 3 storeys
- ii. the maximum distance of the link under any structure is 18m.

8. Laneway/shared way zones

8.1. Explanation

Laneways and shared zones contribute to the fine grain character of urban areas and help to enhance walkability and connectivity. They also provide an important service function for waste management as well car parking access to developments. The establishment of laneways/shared zones is important for both Kensington and Kingsford town centres to achieve an appropriate scale transition and separation from surrounding lower scaled neighbourhoods. Laneways and shared zones can improve the permeability of both Kensington and Kingsford town centres, encouraging walking, enhancing social gathering and activities such as outdoor dining, play and art and culture appreciation.

Objectives:

The objectives for laneway/shared way zones are to:

1. Establish a network of laneways that encourage pedestrian movements and social gathering spaces
2. Provide leafy, green and useable laneways
3. Facilitate vehicular access and servicing to properties fronting Anzac Parade and Gardeners Road.

Controls

- a) Laneways and shared zones are to be provided in accordance with the relevant block diagram (see Part B of this chapter)
- b) Laneways are to be a minimum of 6 metres wide (for larger developments, a carriageway width greater than 6 metres may be required) and shall provide landscaping, lighting and high quality materials and finishes and opportunities for art to enhance the pedestrian environment
- c) Buildings that front lanes shall be articulated to create visual interest and shall incorporate passive surveillance by orienting windows and balconies onto the lane
- d) Ground floor uses fronting lanes shall incorporate openings onto the lane so as to contribute to the enjoyment and activation of the lane including outdoor dining
- e) Applicants are to negotiate Rights of Carriageway with adjoining property owners where required for access
- f) Laneways are to allow visibility along the length of the link and be open to the sky as much as is practicable.

Note

Evidence of the attempt to obtain the adjoining property owner's agreement to the Right of Carriageway is to be submitted as part of the Development Application

Note

Refer to the Roads and Maritime Services Technical Direction 'Design and Implementation of Shared Zones Including Provision for Parking' in the planning and design of shared way zones.

9. Heritage conservation

9.1. Explanation

The retail and commercial centres of Kensington and Kingsford were established during the early twentieth century. Development generally consisted of fine grain commercial frontages focused to Anzac Parade with some housing to the rear and at upper levels.

While many of these buildings no longer exist within the centres, there remains a small number which continue to contribute to the historical 'main street' context. Through their architectural style, scale and detailing (particularly above street awning level) they attest to the area's transition during early twentieth century history to commercial focus and allow for this significant historical layer to be interpreted.

Some unsympathetic alterations such as infilling of traditional inset verandas, applications of paint or render over face brick or stone have hampered recognition of their aesthetic qualities, however, these buildings should be retained and conserved to ensure a sense of historical continuity and to enhance streetscape character so the town centres remain interpretable over time.

Historical context

Early development of Kensington town centre

The area now encompassed by the suburb of Kensington was, in the nineteenth and early twentieth centuries, a swampland traversed by the Lachlan Stream. A water-powered mill on the bank of the Lachlan Stream near what is now Todman Avenue, produced paper, cloth, and later flour from 1814 until 1832. When emancipist Samuel Terry gained title to the surrounding area in 1819, he called his land the 'Lachlan Mills Estate', a name that was retained after he on-sold the land to former convicts Solomon Levey and Daniel Cooper. Daniel Cooper, who in time bought out Levey's share, encouraged industry on his land, attracting workers and their families to the area.

Following the lifting of Sydney Water Reserve restrictions and the stopping of Sydney drawing its water from Lachlan Stream, Samuel Cooper (nephew of former convict Daniel Cooper, and a man who had envisaged a grander future for the land) attracted private capital which formed a syndicate to subdivide the 'Lachlan Mills Estate'. The winning entry for the best design of the new 'Model Kensington Suburb' was 'Rus In Urb' (Country in city) by Vernon, Joseland, Oxley and Mocatte with its principal feature of a main Boulevard for vehicle and foot traffic, and central tramway.

The first subdivision of the Kensington Freehold Estate was in April 1891 by the Australian Cities Investment Corporation Ltd and saw the sale of 96 allotments, including 9 along Anzac Parade (and the whole eastern side of Doncaster Avenue). Another public auction in October 1891 saw the offering of the balance of unsold allotments in the Kensington Freehold Estate, including lots fronting Anzac Parade between Bowral Street and Doncaster Ave, and lots fronting Ascot, Bowral, Todman and Darling Streets. The allotment sizes first established in April were maintained. Land on the western side of Anzac Parade, between Salisbury Road and Grosvenor Street was released later, with auctions continuing until at least March 1906. In October 1907, the Centennial Park Lands on the corner of Alison Road and Anzac Parade were submitted for public auction.

Substantial development of Kensington occurred within in the 1920s, marking an important stage of development along the recently named Anzac Parade. This was boosted by the rapid

increase of residential development, particularly to land within West Kensington (released in 1912).

Shops sprang up on either side of the road, and adjoining the Doncaster Hotel (built 1922-23), the Masonic Temple was built on the south western side of Anzac Parade and became popular for social events, and the Doncaster Theatre was erected between Anzac Parade and Doncaster Avenue.

Early development of Kingsford town centre

Prior to 1900, the area occupied by South Kensington formed part of the Botany Bay Sands. The earliest tracks across the Botany Sand Beds were originally Aboriginal pathways which had traversed along higher ground, which later evolved to form the earliest roads through the area. Bunnerong Road had been an established track providing a connection to Botany Bay.

Settlement of the Kingsford area followed the activation of the Nepean Water Supply Scheme in 1887 and the lifting of bans on development around the Lachlan Swamps and Botany Lakes. However, Bunnerong Road remained substantially undeveloped until the early 1900s, with only scattered residences and trades related to the expanding horse racing industry at Randwick and Kensington racetracks.

Development of Kingsford substantially accelerated after the turn of the twentieth century and following the extension of the Sydney tram service beyond the junction of Randwick and Alison Roads in December 1901 and running from Kensington to Little Bay. The first estate subdivisions at South Kensington began in 1904 and subsequent estate sales in the vicinity of the road junction at South Kensington occurred in 1910, 1915 and 1917. Development was further boosted with the construction of the tramline along Gardeners Road in 1913, as well as the establishment of the Dacey Garden Suburb (the Housing Board estate designed by Sir John Sulman) at the intersection of Bunnerong Road, Gardeners Road and Anzac Parade increasing demand for retail and service buildings within the locality.

Entrepreneur and racing identify Jack O'Dea built the first 'shop residences' at South Kensington on Bunnerong Road, including O'Deas Corner (424-436 Anzac Parade) in 1912, and was also responsible for the construction of over 100 houses in South Kensington prior to 1922. Rows of combined shop residences and groups of free standing dwellings were constructed along the 'village' section of Bunnerong Road, and many of the early houses fronting Bunnerong Road were later converted to shops during the 1920s as a result of the rapid suburbanisation of surrounding streets.

The opening of the Hotel Kensington (1914) designed by Architects, Robertson and Marks, marked the importance of the junction at Bunnerong Road and Rainbow Street and retail frontages further extended around the northern side of the Nine Ways intersection during the 1930s and by the 1940s, Kingsford was characterised by a cohesive setting of retail and commercial frontages defining both sides of Anzac Parade, and returning to the immediate frontages of side streets and cross routes including those to the northern side of the Nine Ways Roundabout.

During the mid-twentieth century, Kingsford went through a further process of change in both character and function. This was highlighted with the construction of the University of NSW on the former Kensington Race Course site at the northern end of Kingsford, a decline in retail buildings due to competition from larger centres, the cessation of tram services in 1961, and the burning down of the Kingsford Odeon cinema. The establishment of the Castellorizian Club on its site reflected the change in the demography of Kingsford with increased European migration after World War 2. It was demolished in the early twenty-first century and replaced by a residential apartment complex. Façade alteration and incorporation of new shopfronts to the early twentieth century and Inter-War buildings continues to occur to support the changing nature of the commercial, retail and restaurant services

Figure 4a: Heritage items and contributory buildings in Kensington town centre



Figure 4b: Heritage items and contributory buildings in Kingsford town centre.



Objectives

The objectives for heritage conservation are to:

1. Conserve and enhance the character and heritage significance of heritage items
2. Retain and conserve distinctive elevations and significant fabric of contributory buildings
3. Encourage sensitive adaptation of heritage items and contributory buildings
4. Ensure infill development is designed to respond sympathetically to the historic built form, character and detailing of nearby heritage items and contributory buildings
5. Ensure that the heritage significance of heritage items and/or conservation areas located in the vicinity of the town centres is considered in the assessment of development applications

Controls

All Development

- a) All development involving heritage items are to be in accordance with requirements for heritage set out in Part B2 of the Randwick DCP
- b) All development involving heritage items and contributory buildings are required to:
 - i) Adhere to the principles of the Burra Charter
 - ii) Include the submission of a Heritage Impact Statement (or Heritage Impact Assessment) which considers the heritage significance of the item or contributory building, the impact of the proposal on the heritage significance of the building or heritage items within the vicinity, the rationale for the proposed development, and the compatibility of the development with the objectives and controls, and/or recommended management within relevant conservation management plans, planning instruments or heritage inventories
- c) Development located within the vicinity of another local government area requires the preparation of a Heritage Impact Statement to address the potential impact on adjoining or nearby heritage items or heritage conservation areas in the adjoining local government area.

Heritage items and contributory buildings

- a) Alterations and additions to heritage items and contributory buildings should conserve original characteristic built form, and not significantly alter the appearance of principal, or historically significant facades, except to remove detracting elements
- b) Alterations and additions to heritage items and contributory buildings should:
 - i) Retain, restore and reinstate (where possible) significant features and building elements to principal elevations, shop fronts and visible side elevations, including, original openings and decorative features such as original doors, windows, sun hoods, awnings, lighting and historic signage
 - ii) Remove unsympathetic alterations and additions, and building elements where possible
 - iii) Retain and encourage adaptive re-use of historic shop fronts and avoid unnecessary screening through planting, signage or other works
 - iv) Retain and conserve the form and articulation of historic street frontages (such as the first structural bay/or first room to preserve inset verandas) and avoid 'facadism'
 - v) Include a minimum 6.5m upper level setback for additions to existing contributory buildings at strategic node sites. A minimum 5.5m upper level setback applies to contributory buildings on all other sites

- vi) Be designed to be clearly distinguishable as new work when undertaking extensions, alterations, reconstruction or repairs
- vii) Incorporate new doors and windows which compatible with the positioning, size and proportions of original windows and doors
- viii) Ensure that conservation works including the reinstatement and restoration of historic fabric is appropriately balanced with the impacts of larger development on the site. Restoration works should enhance the quality of finishes, form and detail
- ix) Incorporate materials, finishes and colours which are visually compatible with the heritage or contributory building and enhance its appearance
- x) Ensure that new services are discretely integrated within and behind retained street frontages and not above awnings
- xi) Introduce new signage to be set below, or no higher than street awning level. Signage above the awning detracts from the detail and quality of historic fabric

New development adjacent to heritage items and contributory buildings:

- c) Development adjacent to heritage items and contributory buildings (infill development) should:
 - i. Be designed to respect the historic scale, proportions and articulation of adjacent contributory built forms, including heights, solid to void ratios and alignments of street awnings
 - ii. Incorporate podiums and framed overlays that reference the principle influence line of historic streetscapes, and are cohesive with the established street frontage
 - iii. Be designed to incorporate setbacks which retain the profile and massing of exposed side elevations to retained contributory built forms
 - iv. Ensure new street elevations maintain the vertical articulation and segmented character of historic building groups which provide variety to the streetscape and sense of human scale, and avoid unrelated horizontally emphasised articulation
 - v. Provide contemporary new signage that compliments the character of the contributory buildings and
 - vi. Ensure that new finishes to side elevations should not detract from street front detailing and finishes
- d) Development should maintain and reinstate the emphasis of street corners and cross routes through reinforcement of historic height lines remaining at, and adjacent to intersections.

PART B

10. Block controls

10.1. Kensington and Kingsford town centre planning review

Explanation

The following section provides block by block envelope controls for Kensington and Kingsford town centres. A building envelope is a three-dimensional space which defines the maximum extent of a building in any direction that is: maximum building height, maximum building length and maximum building depth. Buildings must be designed to fit within the applicable building envelope. 'Flexible Zones' shown on the block diagrams allow for alternative design solutions for internal site configurations and adjoining property interface. Built form outcomes are required to be consistent with the ADG and the maximum height control in RLEP 2012, whilst ensuring suitable height transition to existing lower scale adjoining development.

The building envelopes have been determined by taking into account localised site characteristics, size and orientation, optimum development potential and surrounding built environment context.

They identify the following built form outcomes:

- Distribution of height across each block
- Flexible building zones (shown hatched) for density and height distribution on certain blocks
- Build to lines, ground level setbacks and upper level setbacks
- Active frontages
- Mid-block links and share way/laneways
- Heritage items and contributory buildings
- Preferred vehicular access points (subject to Transport for NSW and Roads and Maritime Services approval); and
- Public domain proposals including plazas.

Alternative design solutions may be considered only where it can be suitably demonstrated that the proposal would result in an improved urban design, amenity and sustainability outcome and meets the identified desired future character for the block.

These block by block controls should be read with other relevant controls for Kensington and Kingsford town centres in this section of the DCP.

A digital 3D Model must be submitted with all DAs in accordance with Council's 3D Model Submission Requirements. An 'as built' 3D Digital Model must be submitted prior to the issue of any Occupation Certificate.

A physical model built to a scale of 1:500 shall be submitted with the following types of applications within the K2K corridor and land immediately adjoining the corridor:

- i. All development greater than 15m in height
- ii. Strategic node sites.

Figure 5a: Block location map Kingsford town centre



Figure 5b: Block location map Kensington town centre



10.2. Strategic node sites

Todman Square Precinct, Kensington

Desired future character

Todman Square Precinct comprises the four strategic node sites referred to as K1, K2, K3 and K4 which occupy the four corners of the Todman Avenue and Anzac Parade junction, adjacent to the Todman Ave light rail stop.

Future redevelopment of the Precinct will form the new ‘heart’ of the Kensington Town Centre. A mix of residential, retail, commercial and cultural uses and activities will be clustered around the Todman Ave Light Rail Stop, leveraging upon excellent accessibility to major employment hubs including the Sydney CBD, and the Randwick Collaboration Area, as well as east-west linkages to surrounding residential areas.

The Precinct will have a lively creative arts and innovation focused environment. A new multi-functional creative space at K1 will provide local opportunities for artistic, cultural and creative expression, while improving public accessibility and opportunities for the community to experience the arts. An innovation space at K3 will accommodate start-ups, co working hubs and incubators with strong synergies to the University of NSW. A variety of arts and non-arts related businesses including cafes, restaurants, small bars, and shops will be clustered around these cultural and innovation anchors.

Todman Square will achieve high quality urban design and amenity, with slender taller articulated buildings of up to 18 storeys in height defining the corner sites, reflecting architectural design excellence and emphasising this new landmark location. The Precinct will have a pedestrian focus with integrated public spaces, generous setbacks along Anzac Parade, active ground street frontages and wider footpaths to accommodate increased pedestrian capacity and footpath dining. The Precinct will facilitate permeability with activated mid-block links and shared laneways embellished with public art and greenery, allowing people to easily navigate within a high amenity and safe environment.

A new plaza at K1 with public art, landscaping and furniture will provide the opportunity for people to meet, interact and connect. Green linear links to an expanded Kokoda Park and the Randwick Racecourse Urban Forest will further foster the liveability and environmental amenity of this Precinct.

Objectives

The objectives for strategic node sites are to:

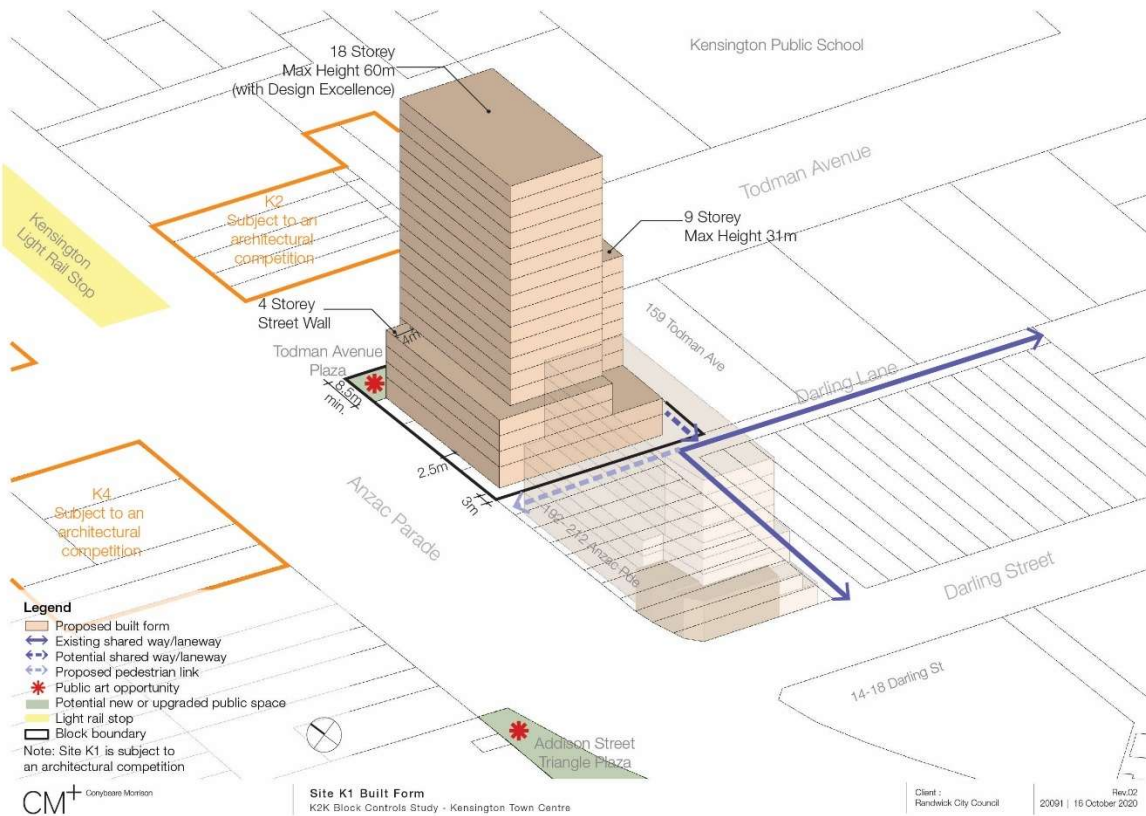
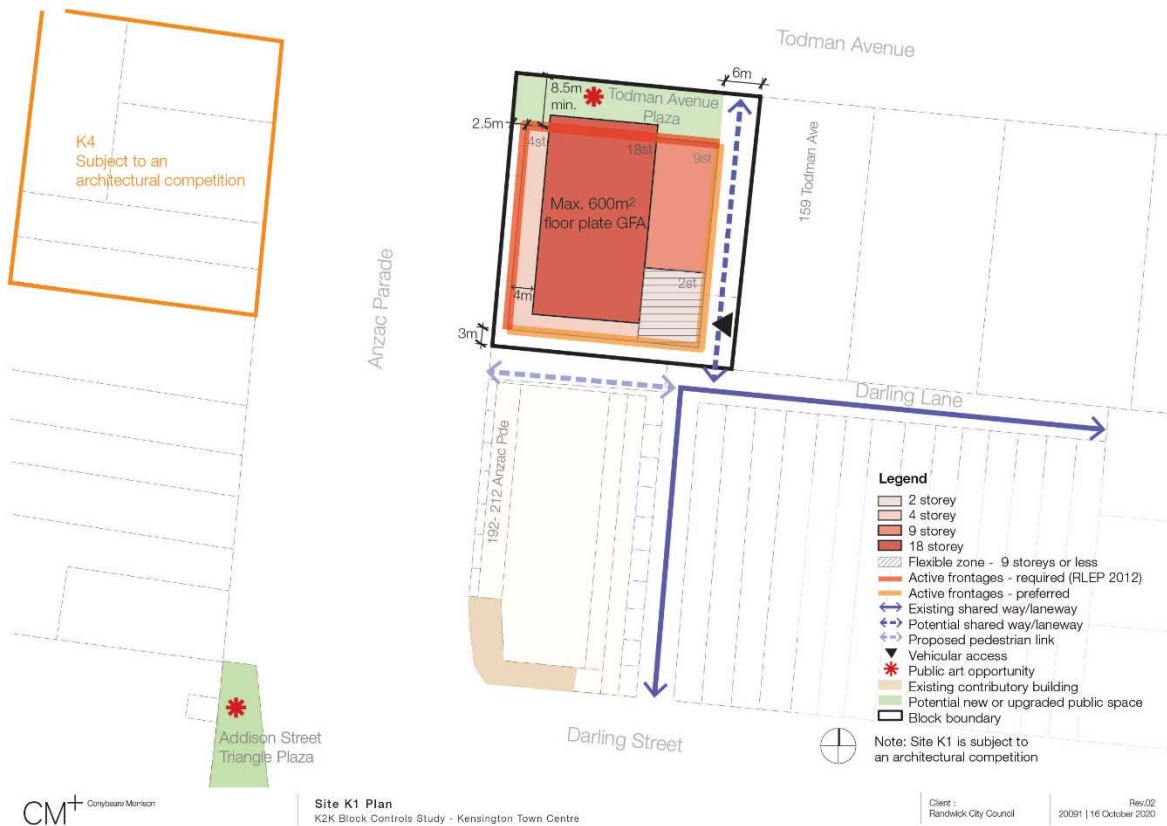
1. Provide for development that supports and encourages the growth of the Precinct as a hub for arts, innovation, cultural and creative endeavours
2. Encourage high quality-built form outcomes and achieve design excellence
3. Ensure new development results in a cohesive urban design outcome and high-quality streetscape across the Todman Square Precinct
4. Locate mixed use development with good access to retail, public transport, employment, high quality public domain and public transport
5. Ensure that built form features articulation and an attractive composition of building elements with a strong relationship between buildings and the streetscape
6. Provide high quality public domain elements, including mid-block links, widened footpaths, public art, street trees, landscaping and a welcoming public plaza
7. Support a thriving day and night-time economy
8. Minimise any adverse impacts on the amenity of the adjoining residential area.

Controls

- a) Future built form at Todman Square must be consistent with the applicable block envelope controls shown in Figures 6, 7, 8 and 9
- b) DAs for strategic node sites are to be undertaken in accordance with the winning design of the architectural design competition
- c) A minimum non-residential floor space is to be provided in accordance with Clause 6.17 of the RLEP 2012
- d) Buildings are to respond to the site's context to provide visual interest and minimise and mitigate potential for overshadowing and privacy impacts upon surrounding land uses
- e) Buildings are to be well articulated and respond sensitively to nearby heritage and contributory buildings in accordance with the requirements under section 9 Part A of this Chapter of the DCP
- f) Buildings are to ensure a cohesive urban design outcome across the Todman Square Precinct in terms of built form, scale and massing and contribute to a high quality streetscape environment
- g) Built form within 'Flexible Zones' is to be designed to comply with the maximum building height in the RLEP 2012, objectives of this clause and the requirements of the ADG to achieve a suitable transition to adjoining lower scale development
- h) Continuous active street frontages on the ground floor and adjacent to laneways are to be provided in accordance with the requirements of section 19 Part C of this chapter of the DCP
- i) A multi-functional creative space with a minimum floor area of 200m² is to be provided at the K1 site and dedicated to Council. Floor area for the creative space is to be excluded from the total gross floor area of the site
- j) A public plaza is to be provided immediately north of the K1 site in accordance with Figure 6
- k) An innovation hub with a minimum floor area of 200m² is to be provided at the K3 site and dedicated to Council. Floor area for the innovation hub is to be excluded from the total gross floor area of the site
- l) Green walls, roofs and landscaping is to be provided in accordance with section 21 Part C of this chapter of the DCP.

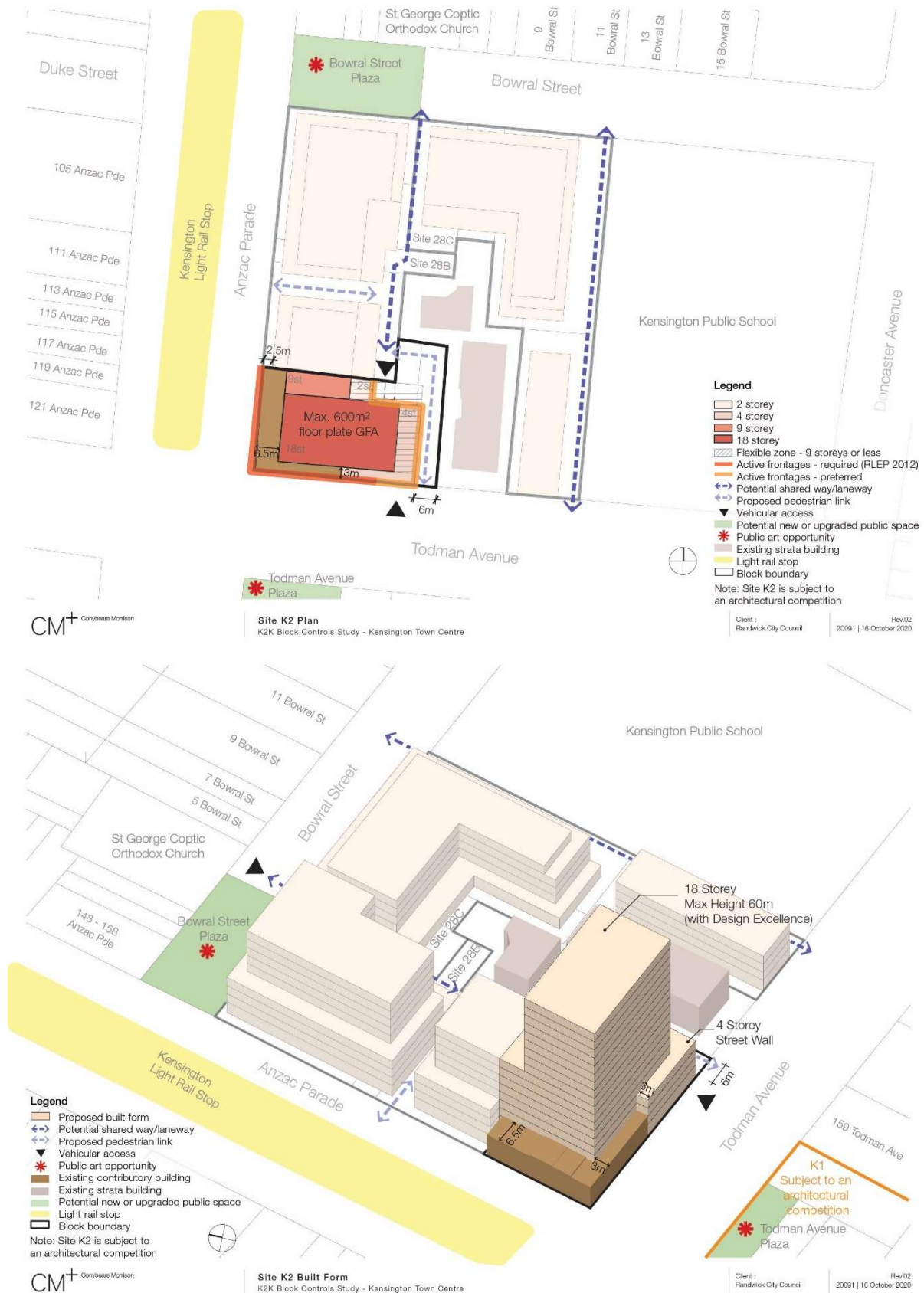
K1 Site

Figure 6: K1 Site block controls, Todman Square



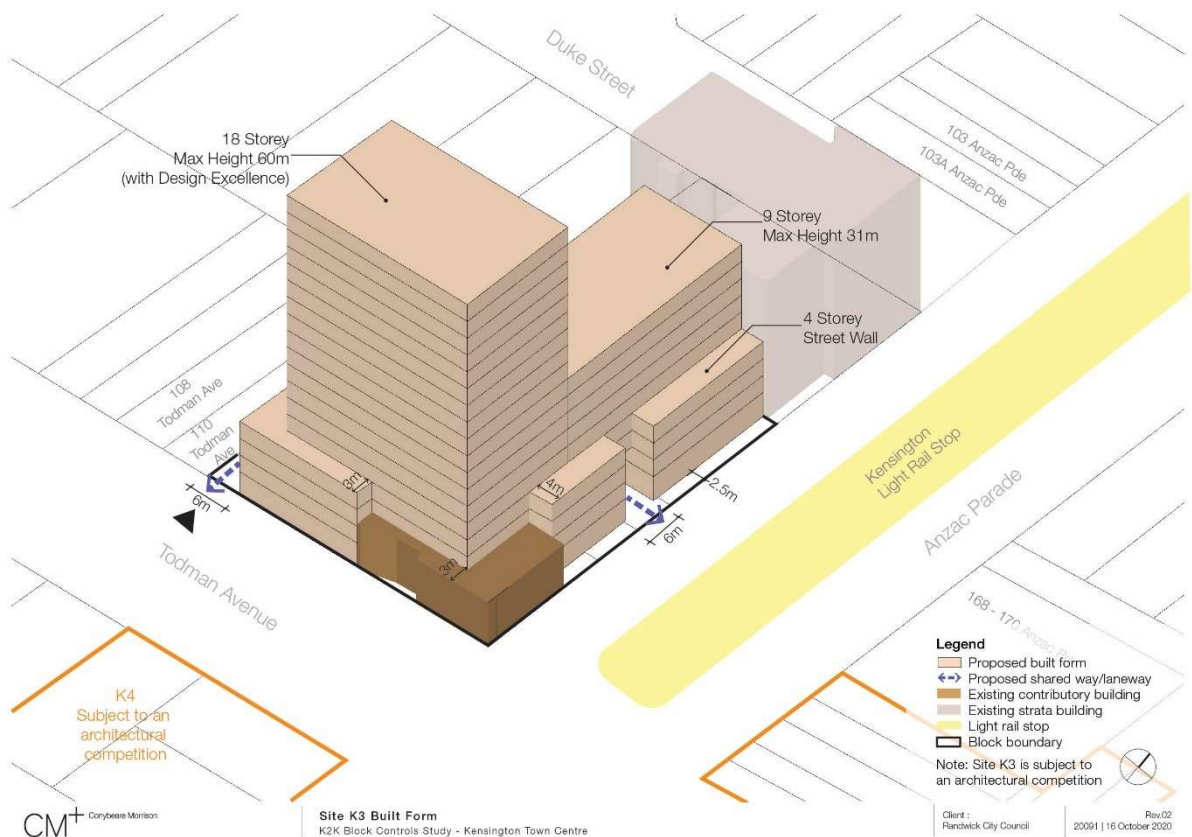
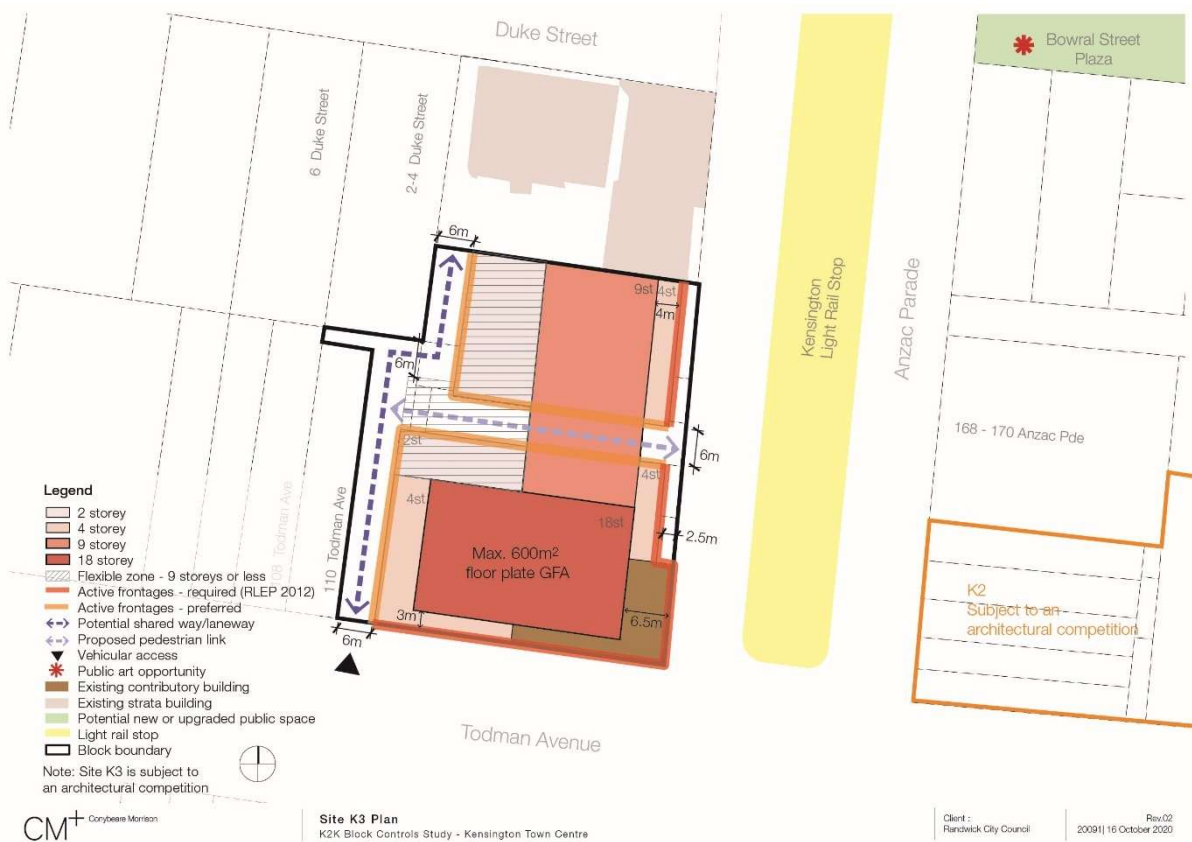
K2 Site

Figure 7: K2 Site block controls, Todman Square



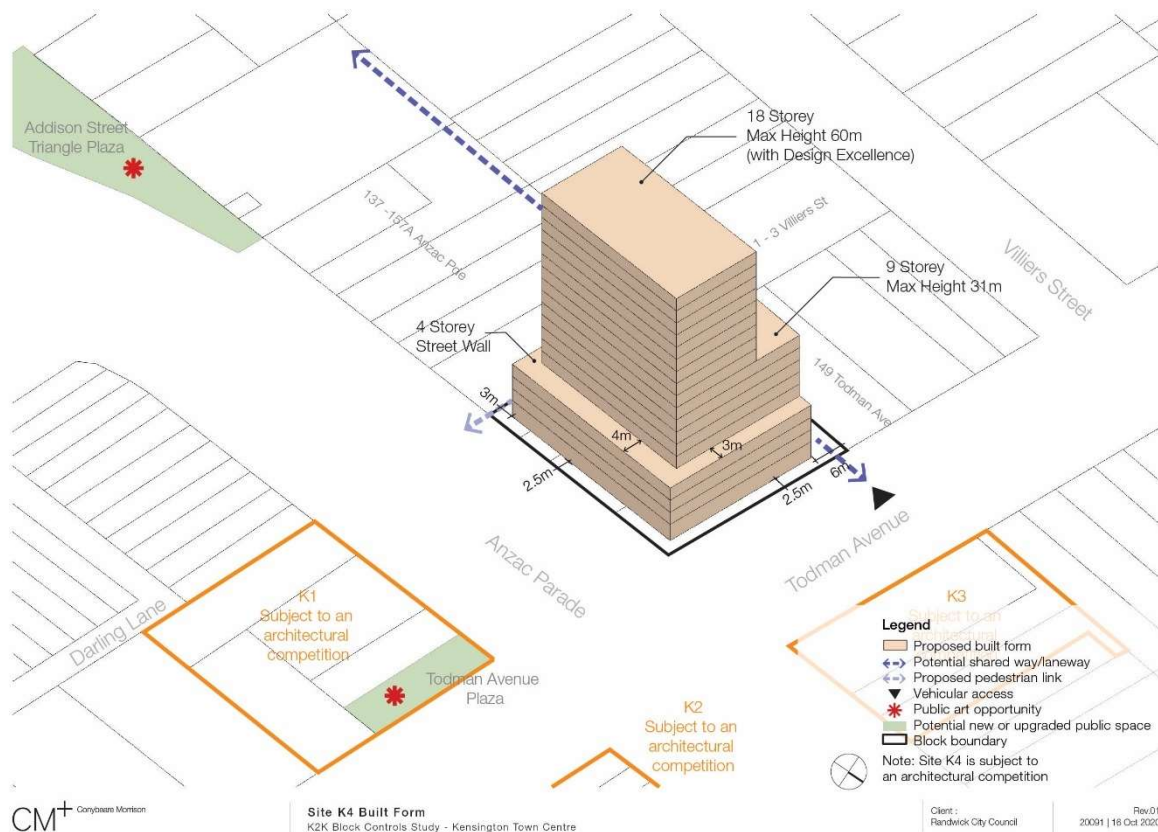
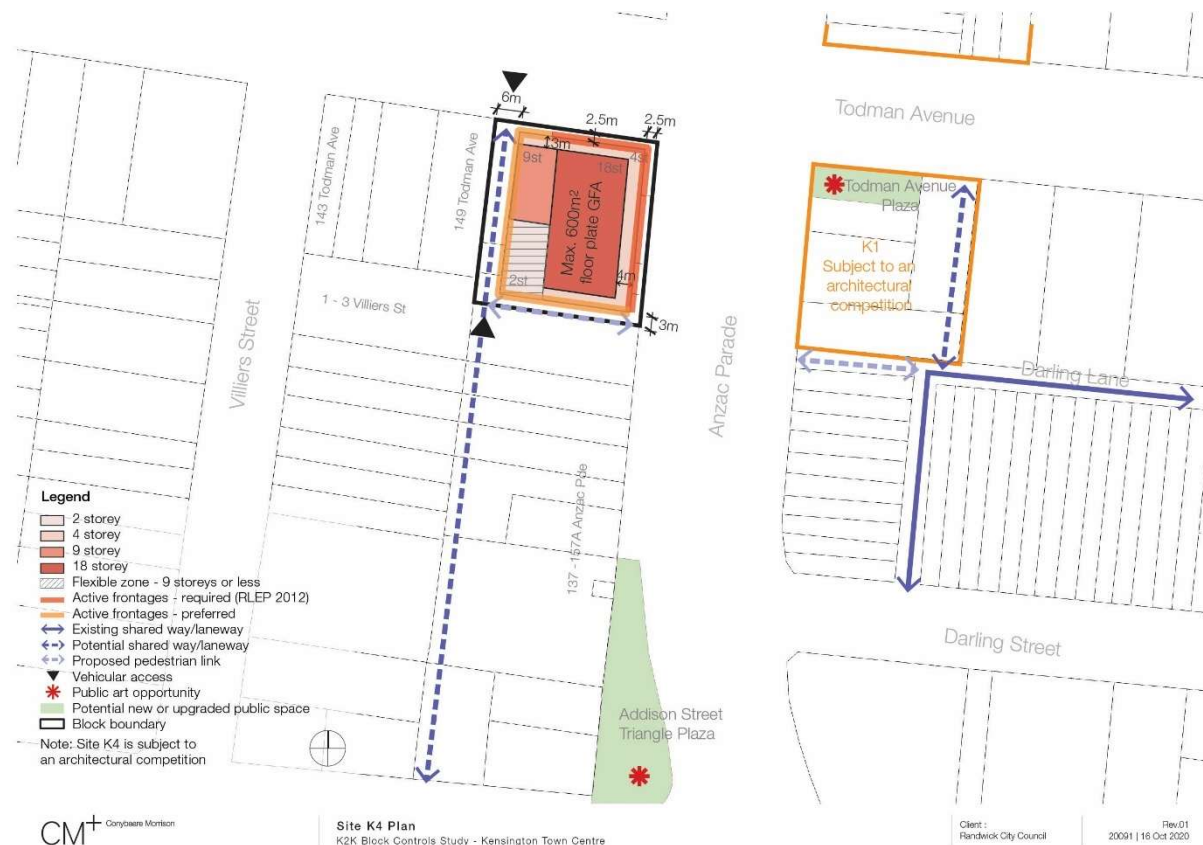
K3 Site

Figure 8: K3 Site block controls, Todman Square



K4 Site

Figure 9: K4 Site block controls, Todman Square



Kingsford Junction Precinct

Desired Future Character

Kingsford Junction comprises the Kingsford Triangle (2A and 2B) and the Rainbow Street (3A and 3B) sites located on the southern end of the Kingsford Town Centre. The Precinct occupies a prominent location at the convergence of key streets adjacent to the Juniors Kingsford Light Rail Terminus. The Precinct's triangular geometry provides a significant opportunity to establish a gateway to the Kingsford town centre that is iconic, well connected and activated.

Future development of Kingsford Junction is to provide for a variety of residential, commercial, retail and community uses clustered around the light rail terminus, making greater use of the public transport network and high level of accessibility. New development is to respond to the unique landmark setting of Kingsford Junction and focus on establishing active edges and integrating built form with the surrounding context. It also requires a design response that is sensitive to the surrounding heritage context including Dacey Gardens and Daceyville heritage conservation area to the south-west and contributory buildings at Maloney's Corner.

The Kingsford Triangle and Rainbow Street sites are large strategic node sites and are expected to accommodate slender articulated landmark tower forms of 17 storeys reflecting architectural design excellence and best practice in sustainability. The building form is to be stepped down from 17 to 9 and 7 storeys at the Kingsford Triangle site, to achieve a scale transition to surrounding residential neighbourhoods and reduce building bulk and overshadowing. A similar scale transition will be achieved on the Rainbow Street site to minimise visual amenity and privacy impacts for surrounding areas.

A civic scaled environment will be created through four storey street walls integrated into built form, together with generous setbacks and wider footpaths. A high level of connectivity and permeability through and around the site will be achieved via open air mid-block links which open up public access corridors to surrounding streets.

The public realm will be green and inviting through the provision of large canopy trees, public plazas, landscaping, seating and interactive public art. A new public plaza at the Rainbow Street site will provide a focal point for civic pride and community expression, encouraging people to linger, interact and connect. A community hub at the Rainbow Street site will provide a mix of uses such as a 'one stop shop' of consolidated office space, community services and facilities as well as public parking.

Objectives

The objectives for the Kingsford Junction Precinct are to:

1. Ensure design excellence and provide for redevelopment that addresses the desired future character of the Precinct
2. Ensure new development responds to the context and visual setting of the Precinct providing a distinct gateway to the Kingsford town centre
3. Provide tall landmark buildings at the Kingsford Triangle and Rainbow Street sites that define the Precinct and exhibit innovation in design and sustainability
4. Provide a well balanced mix of uses and scales suitable to the site's location on the southern fringe of Kingsford town centre
5. Support the Precinct's role as a significant transport interchange in the locality
6. Provide high quality plazas and civic spaces that encourage community interaction, passive recreation and socialisation
7. Activate the ground floor public domain to create a vibrant Precinct that is active day and night
8. Improve site permeability and connectivity via mid-block links and pedestrian accessways

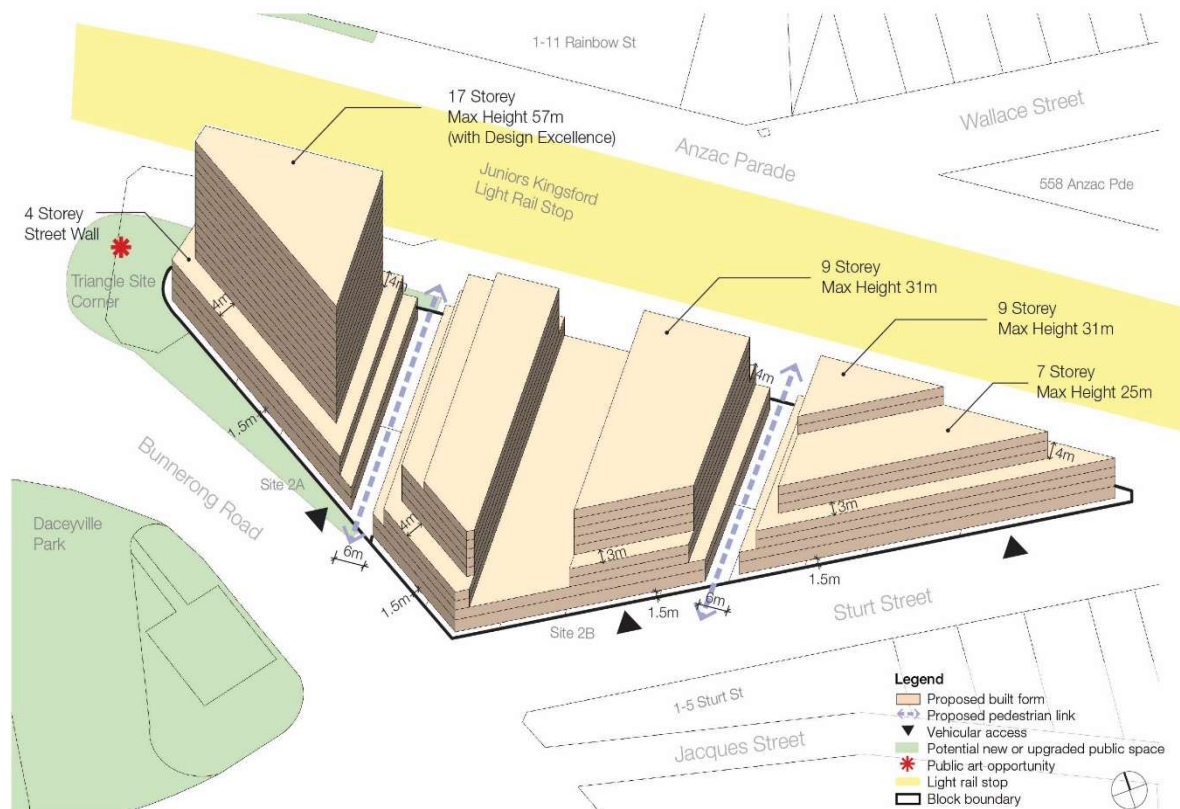
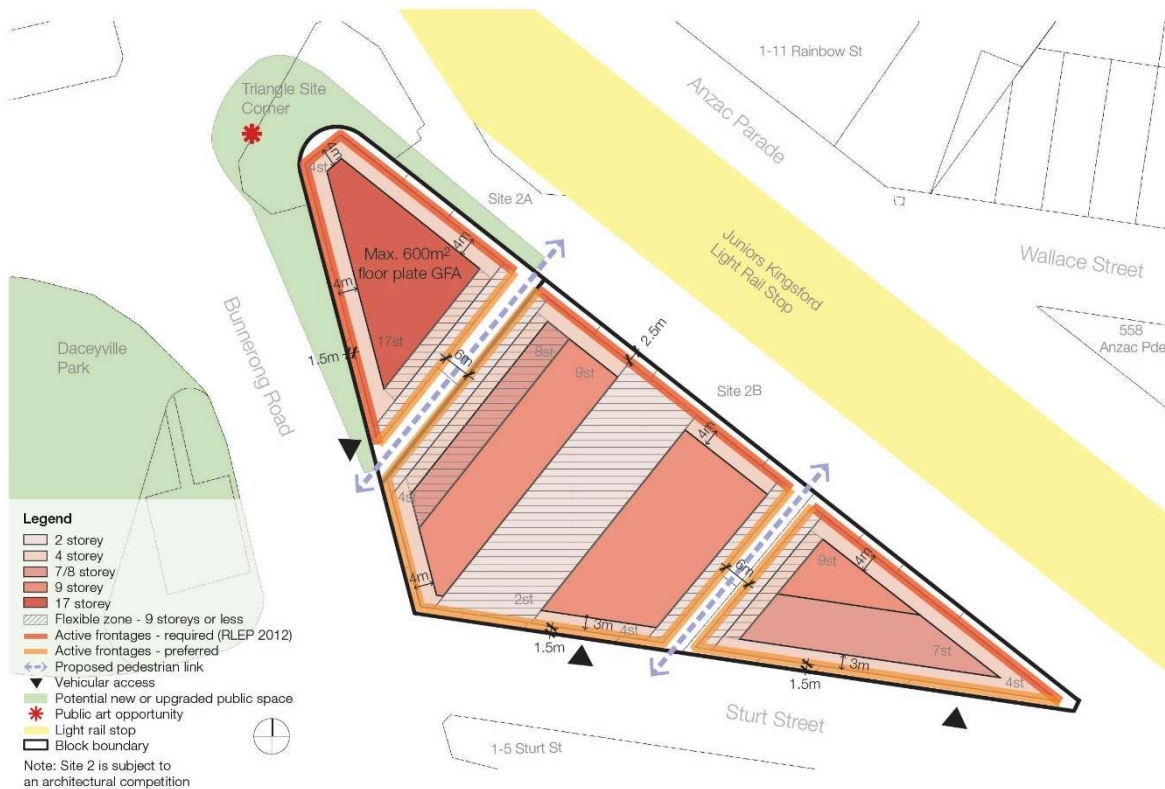
9. Enhance the public realm through generous setbacks, landscaping, mature trees, and footpath verges incorporating water sensitive urban design
10. Respect and protect the amenity of existing uses on the block that are unlikely to change
11. Minimise any potential adverse impacts on nearby heritage uses, in particular Dacey Gardens and Daceyville heritage conservation area
12. Establish a community hub for the delivery of social, cultural, and educational services and programs.

Controls

- a) Future built form at Kingsford Junction must be consistent with the applicable block envelope controls shown in figures 10 and 11, including heights, setbacks, street walls, and mid-block links
- b) DAs for strategic node sites are to be undertaken in accordance with an architectural design competition
- c) A minimum non-residential floor space is to be provided in accordance with clause (to be inserted) in the RLEP 2012
- d) Buildings are to respond to the context to provide visual interest and minimise and mitigate potential for overshadowing and privacy impacts upon surrounding land uses
- e) Built form within 'Flexible Zones' is to be designed to comply with the maximum building height in the RLEP 2012, objectives of this clause and the requirements of the ADG to achieve transition to adjoining lower scale development
- f) New development is to be well articulated and respond sympathetically to Dacey Gardens, Daceyville heritage conservation area and nearby contributory facades, particularly in terms of curtilage, views and setting
- g) Continuous active street frontages on the ground floor and adjacent to mid-block links are required in accordance with section 19 Part C of this chapter of the DCP
- h) Development must demonstrate interface solutions to the Kingsford Light Rail Terminus including generous setbacks to increase pedestrian carrying capacity
- i) A plaza is to be provided at the Rainbow Street site incorporating the following design features:
 - a minimum area of 1500m²
 - a scale and configuration that complements the streetscape and is well integrated with development on the site
 - bordered by active frontages and easily accessible for all members of the public
 - outdoor seating, public art, quality landscape and creative lighting elements
 - water sensitive urban design that integrates with landscaping to capture and manage stormwater
- j) A multi-functional community hub with a minimum gross floor area of 200m² is to be provided at the Rainbow Street site, preferably on the ground floor accessible from the public space/plaza. The multi-functional community hub is to be dedicated to Council.

Site 2A/2B Kingsford triangle block

Figure 10: Kingsford Triangle block controls, Kingsford Junction



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Site 2 Built Form
K2K Block Controls Study - Kingsford Town Centre

Client:
Randwick City Council

Rev:01
2020/1 | 16 October 2020

Site 3A/3B Rainbow Street block

Figure 11: Rainbow Street block controls, Kingsford Junction

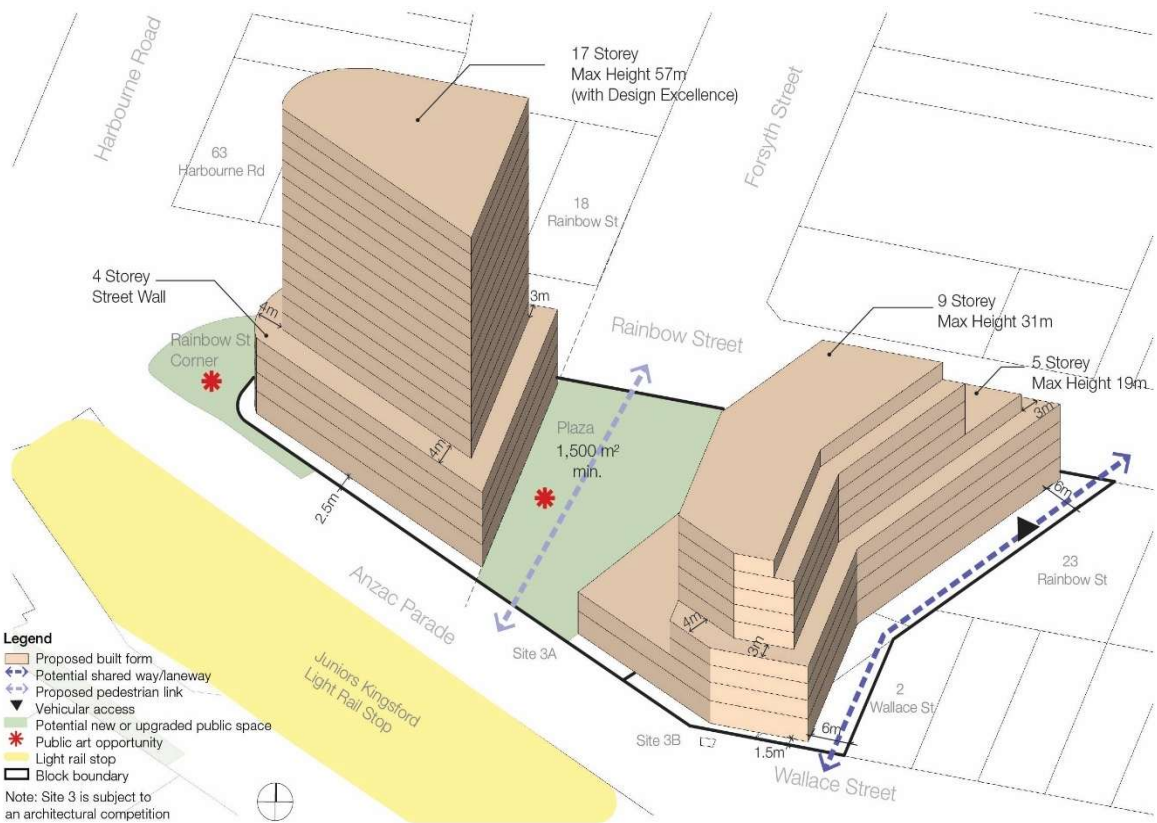


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Site 3 Plan
K2K Block Controls Study - Kingsford Town Centre

Client: Randwick City Council

Rev:01
2009/1 | 16 Oct 2020



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Site 3 Built Form
K2K Block Controls Study - Kingsford Town Centre

Client: Randwick City Council

Rev:01
2009/1 | 16 Oct 2020

Kingsford midtown precinct

Desired future character

The Kingsford Mid-Town Precinct is located in the old heart of Kingsford town centre and comprises the strategic node sites referred to as K5, K6 and K7.

Built form and building heights in this precinct will be carefully considered to achieve good amenity and respond to surrounding historic buildings, while ensuring a fine grain character is retained.

Tall, slender well-articulated buildings at K5, K6 and K7 will be generally up to 18 storeys in height, exhibiting design excellence and emphasising the mid-town role of this Precinct. Buildings on the eastern and western edge of the Precinct will be scaled down to 5 storeys in conjunction with a 2m ground floor building setback to encourage mews style development and achieve a height transition to adjoining lower scale neighbourhoods.

New development will reinforce a four-storey street wall along Anzac Parade to facilitate a fine grain civic scale. A minimum 2.5m building setback along Anzac Parade will create a comfortable pedestrian environment enhanced by awnings, street trees, landscaping, lighting and street furniture.

The historic fabric of the Precinct will be respected and celebrated with new built form designed to achieve a harmonious relationship with historic buildings, including O'Deas Corner, and nearby contributory buildings in terms of scale, form and detailing. An upper level setback of 6.5m for contributory buildings will maintain the form and articulation of historic frontages.

Kingsford Midtown will be a focus for innovation, cutting edge design and sustainability. The provision of an innovation hub at K5 will accommodate start-ups, incubators and creative industries, fostering strong synergies with the University of NSW nearby.

The Precinct will continue to maintain the strong convenience retail and dining role of the wider Kingsford Town centre, reflected by a diverse range of shops, cafes and restaurants. Active uses on the ground floor of buildings along Anzac Parade and Strachan Street will increase the vibrancy of the Precinct and wider town centre.

The precinct will have a vibrant evening economy, building up upon its distinct Asian dining character with active shop fronts, high quality public realm and street life that encourages people to mingle and meet. Footpath dining will be encouraged along Strachan Street with footpath widening and street trees providing a high level of amenity.

The greening of this Precinct with boulevard trees, landscaping and linear links to Kensington Park will contribute to the liveability of this Precinct, making it a place where people want to live, work and visit.

Objectives

The objectives for the Kingsford midtown precinct are to:

1. Ensure design excellence and provide for redevelopment that addresses the desired future character and cohesiveness of the precinct
2. Provide taller landmark buildings that respond sensitively to the scale, proportions, form and detailing of nearby heritage, contributory buildings and other properties
3. Provide for high quality development comprising a mix of uses including commercial, residential, innovation spaces, retail and cultural facilities
4. Ensure the Anzac Parade façade retains a human scale with strong vertical articulation and fine grain character
5. Provide excellent pedestrian amenity through continuous awnings and high quality well landscaped public domain.

Controls

- a) Future built form at Kingsford Midtown must be consistent with the block envelope controls shown in figures 12, 13 and 14, including heights, setbacks, street walls, and mid-block links
- b) DAs for strategic node sites are to be undertaken in accordance with an architectural design competition
- c) Buildings are to respond to the context to provide visual interest and minimise and mitigate potential for overshadowing and privacy impacts upon surrounding land uses
- d) Built form is to respond sensitively to O'Deas Corner, and other contributory buildings in accordance with the requirements outlined in section 9 Part A of this chapter of the DCP
- e) Built form within 'Flexible Zones' is to be designed to comply with the maximum building height in the RLEP 2012, objectives of this clause and the requirements of the ADG to achieve transition to adjoining lower scale development
- f) An innovation hub with a minimum area of 200m² is to be provided at the K5 site, either on the ground or first floor and dedicated to Council
- g) A minimum non-residential floor space is to be provided in accordance with clause (to be inserted) of RLEP 2012
- h) Green walls, roofs and landscaping is to be provided in accordance with the requirements outlined under section 20 Part C of this chapter of the DCP
- i) Continuous active street frontages on the ground floor and adjacent to laneways are to be provided in accordance with section 19 Part C of this chapter of the DCP
- j) Buildings are to ensure a cohesive urban design outcome across the Kingsford Mid-Town Precinct in terms of built form, scale and massing and contribute to a high quality streetscape environment.

Site K5: Kingsford Midtown

Figure 12: K5 Site block controls, Kingsford Midtown

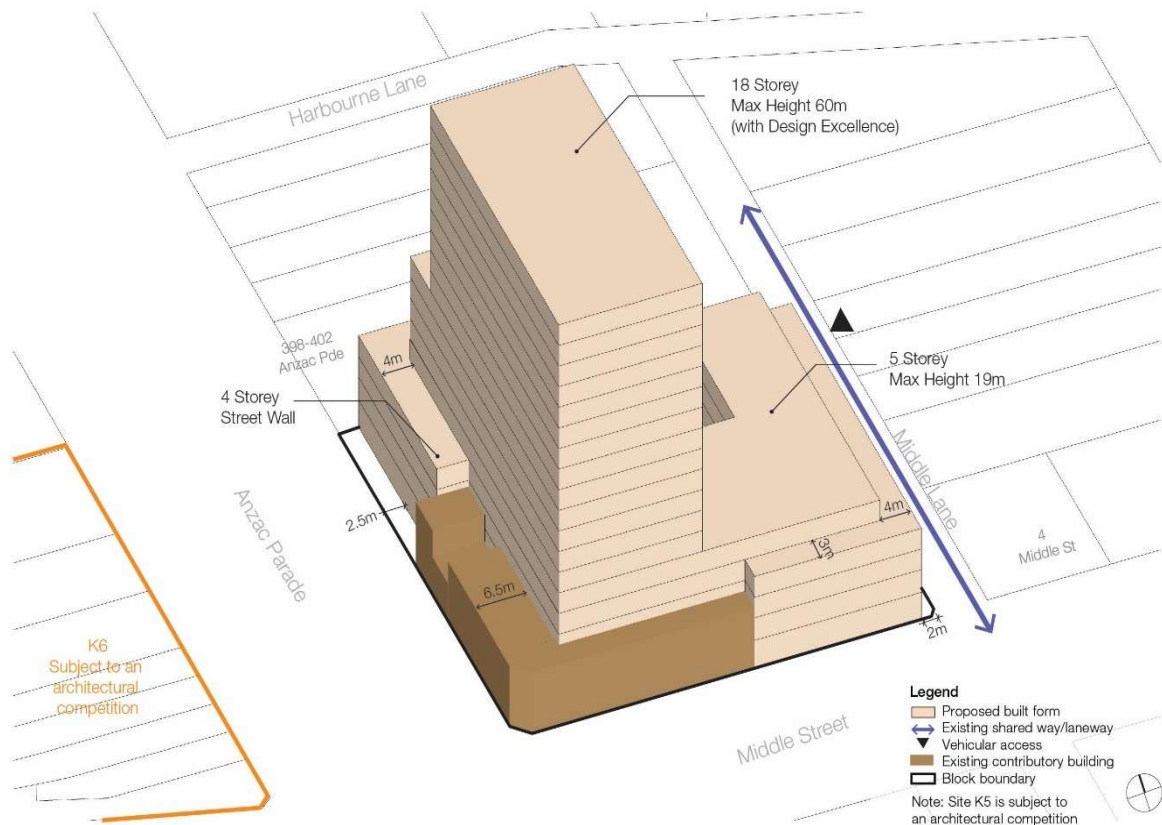


CM⁺ Conybeare Morrison

Site K5 Plan
K2K Block Controls Study - Kingsford Town Centre

Client :
Randwick City Council

Rev:02
20091 | 16 October 2020



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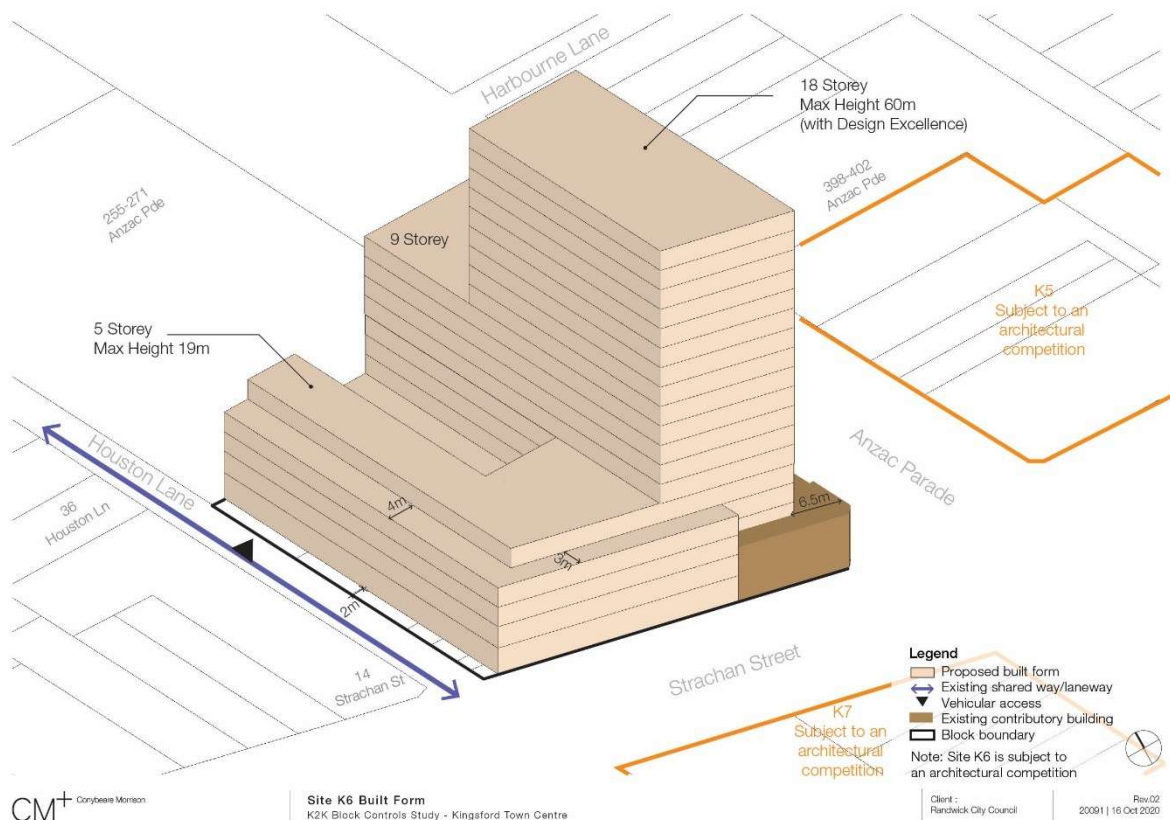
Site K5 Built Form
K2K Block Controls Study - Kingsford Town Centre

Client :
Randwick City Council

Rev:02
20091 | 16 October 2020

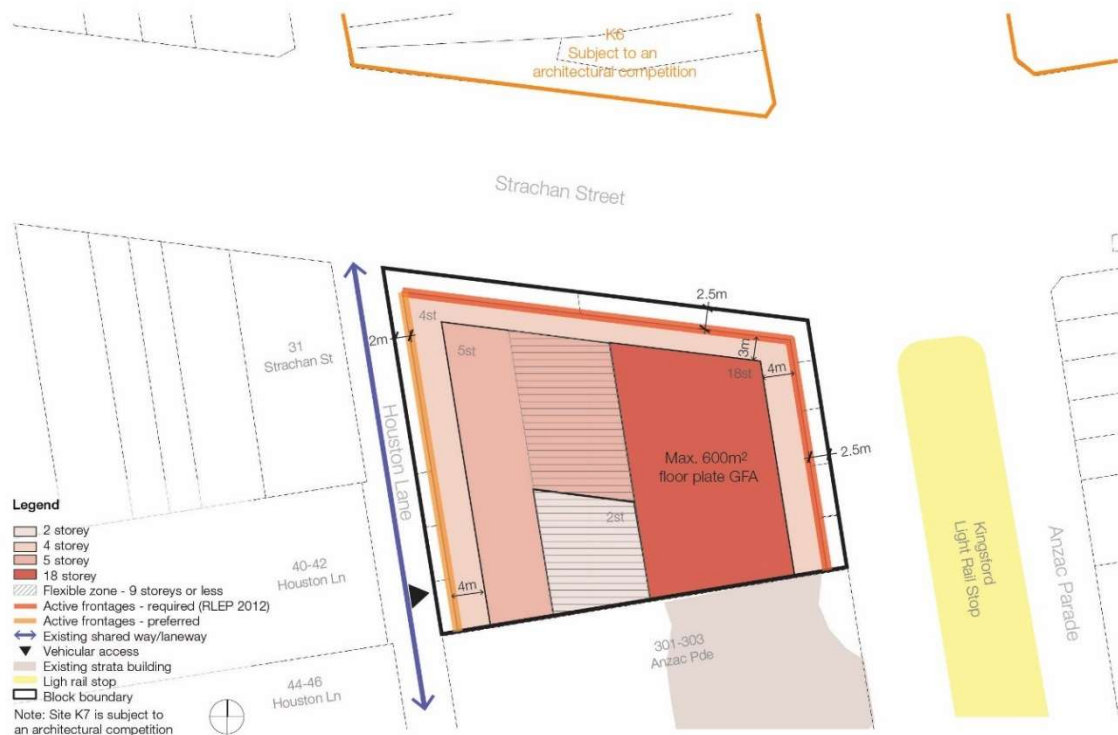
Site K6: Kingsford Midtown

Figure 13: K6 Site block controls, Kingsford Midtown



Site K7: Kingsford Midtown

Figure 14: K7 Site block controls, Kingsford Midtown

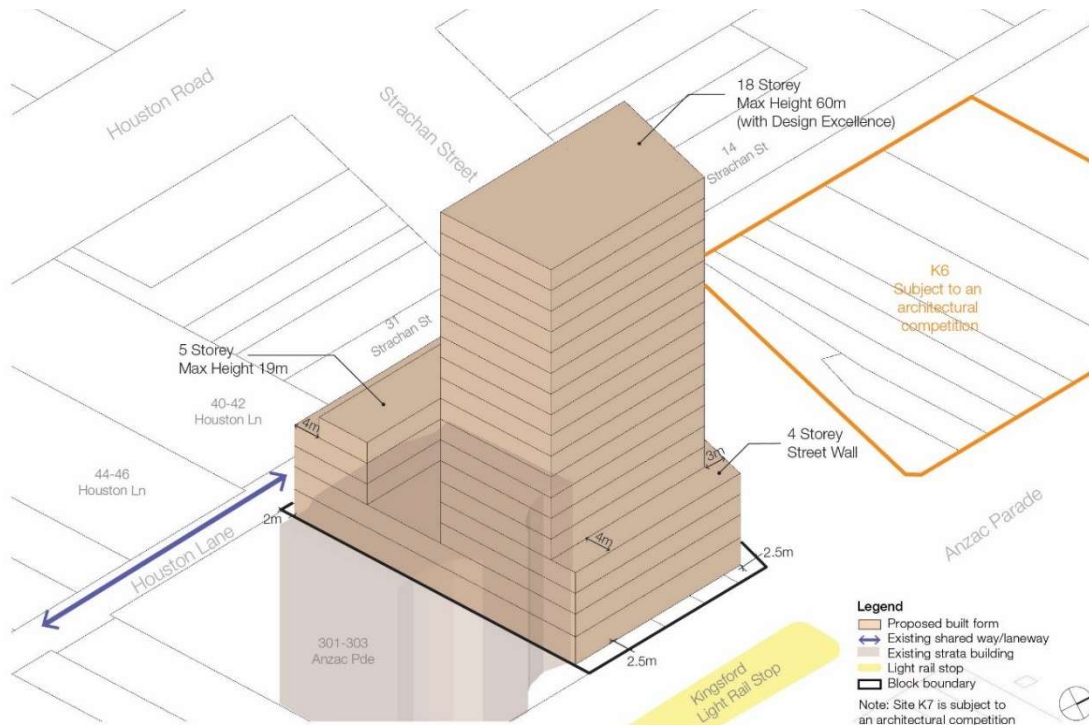


CM⁺ Conyers Morrison

Site K7 Plan
K2K Block Controls Study - Kingsford Town Centre

Client:
Randwick City Council

20091 | 16 October 2020
Rev:01



CM⁺ Conyers Morrison

Site K7 Built Form
K2K Block Controls Study - Kingsford Town Centre

Client:
Randwick City Council

20091 | 16 October 2020
Rev:01

10.3. Block by block controls - other sites

The following section contains general objectives and block envelope controls for other sites within the Kensington and Kingsford town centre. It should be read in conjunction with the objectives and controls relating to the wider Kensington and Kingsford town centres and other parts of the Randwick City DCP. If there is an inconsistency between this part of the DCP and other parts of the Randwick City DCP, this part of the DCP will prevail.

Objectives

The objectives for block by block controls are to:

1. Provide a mix of uses that support the economic prosperity and liveability of the Kensington and Kingsford town centres
2. Facilitate high quality built form outcomes that demonstrate design excellence and amenity
3. Establish an appropriate scale, dimensions, form and separation of buildings
4. Protect and enhance amenity between adjoining developments in terms of solar access and privacy
5. Create a height transition between the centre and the surrounding residential area.
6. Improve connectivity and permeability within the block structure
7. Ensure new development presents a human scale urban edge to the public realm.

Controls

- a) Development must be consistent with the relevant block envelope controls including heights, setbacks, street walls, mid-block links and laneways
- b) Built form within 'Flexible Zones' is to be designed to comply with the maximum building height in the RLEP 2012, objectives of this clause and the requirements of the ADG to achieve transition to adjoining lower scale development.

Kingsford Town Centre

Block 1

Figure 15a: Block 1



Desired future character

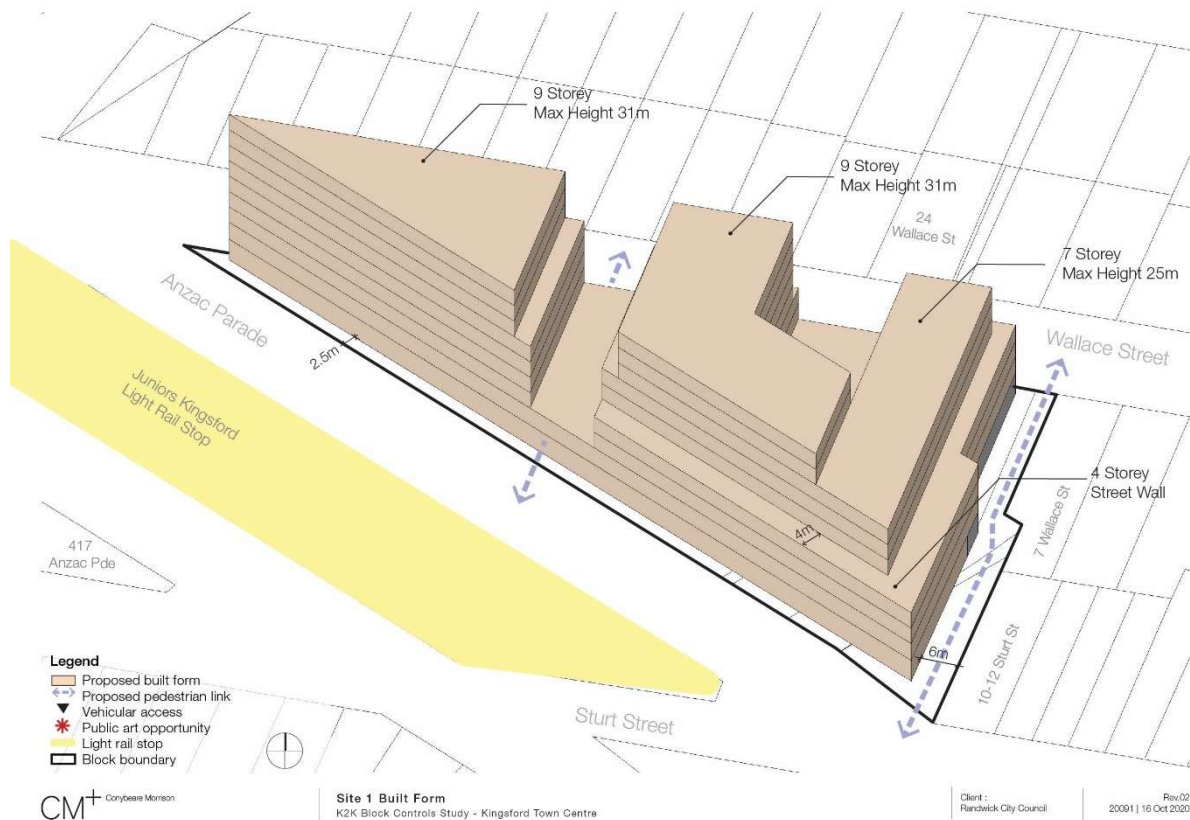
This triangular shaped block occupies a prominent location adjacent to the Kingsford Light Rail Terminus (Juniors Kingsford Stop) south east of the Kingsford Junction Precinct. It currently contains the South's Junior's Club (4 storey development) and a small row of 2 storey multi business properties. To the east is a 2 storey residential flat building situated on the Stuart Street frontage.

The preferred outcome for the block is to ensure buildings address both Anzac Parade and Wallace Streets. The site's geometry, change in topography between Wallace Street and Anzac Parade and strategic location provides the opportunity to establish a distinctive built form outcome at the corner of Anzac Parade and Wallace Street utilising strong vertical emphasis, façade articulation and architectural roof features. A mid-block pedestrian link is required given the length of this block to provide convenient access to the light rail stop. The mid-block pedestrian link is to be either open to the sky or under a building (greater than 3 storeys high). The location of the mid-block link is flexible and is to be resolved as part of the overall design across the whole block with active shop frontages along its length on both sides.

Height will be distributed across the block, stepping down from 9 storeys at the Anzac Parade/Wallace Street corner to 9 and 7 storeys to achieve a scale transition from the mixed use character of the town centre to the residential development along Wallace and Stuart Streets.

Increased ground floor building setbacks along the Anzac Parade frontage will improve the carrying capacity of the footpaths close to the Light Rail Terminus, and together with active street frontages and a 4 storey street wall will enhance the civic scale to the precinct. A laneway at the eastern boundary of the block will provide pedestrian access to the block as well as separation from the lower scaled residential neighbourhood to the east.

Figure 15b: Block 1



Block 2

Note

Refer to Kingsford triangle site (2A/2B) provisions

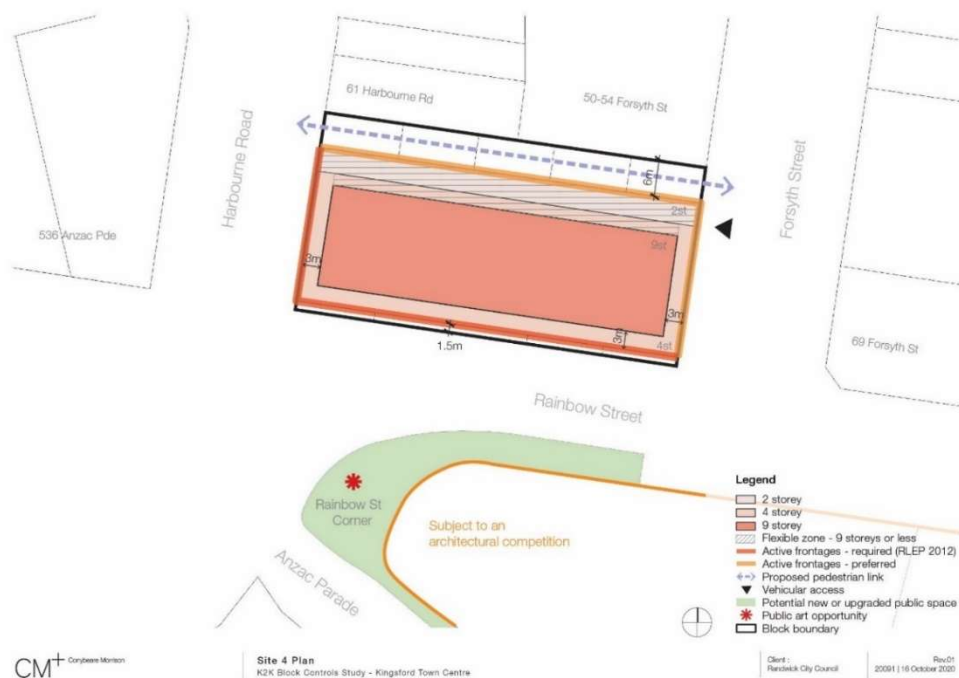
Block 3

Note

Refer to Rainbow Street site (3A/3B) provisions

Block 4

Figure 16a: Block 4



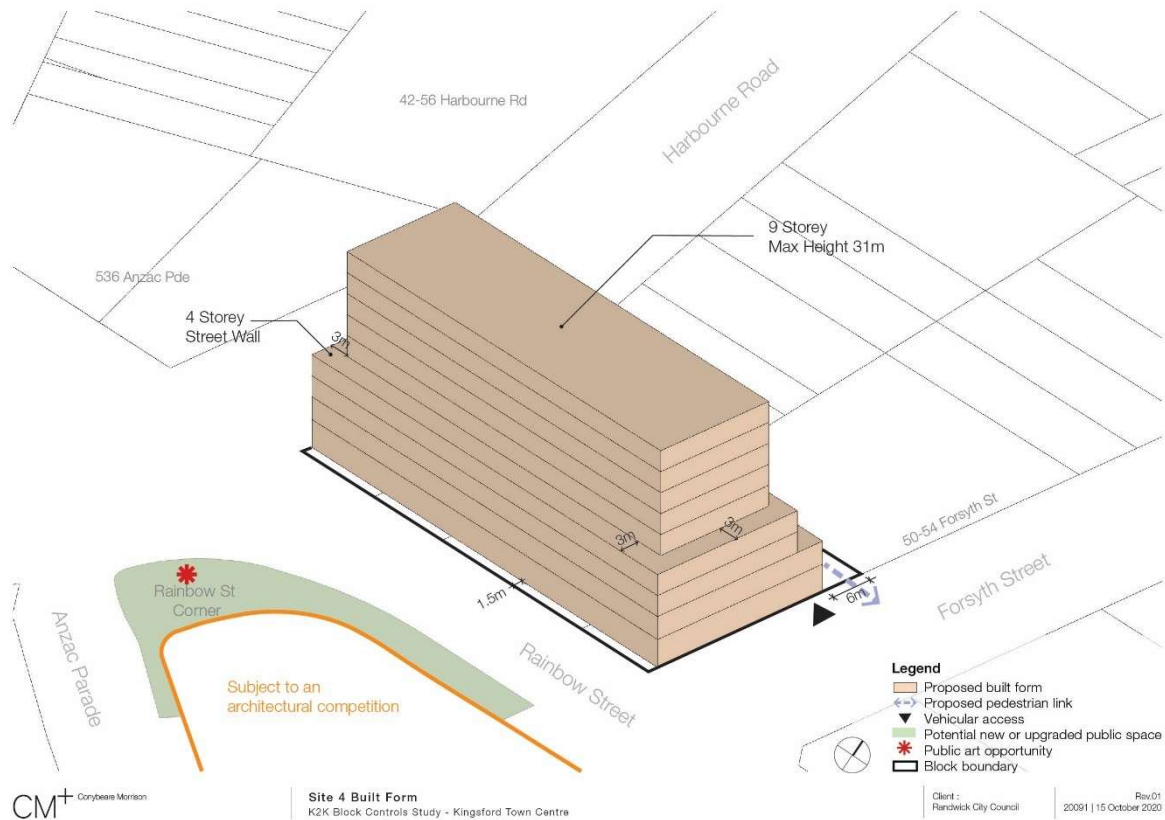
Desire future character

The block is located in a prominent position bounded by Harbourn Road, Forsyth Street and Rainbow Street, north-east of the Kingsford Junction. It is currently occupied by 3 residential flat buildings ranging from 2- 4 storeys and a commercial building.

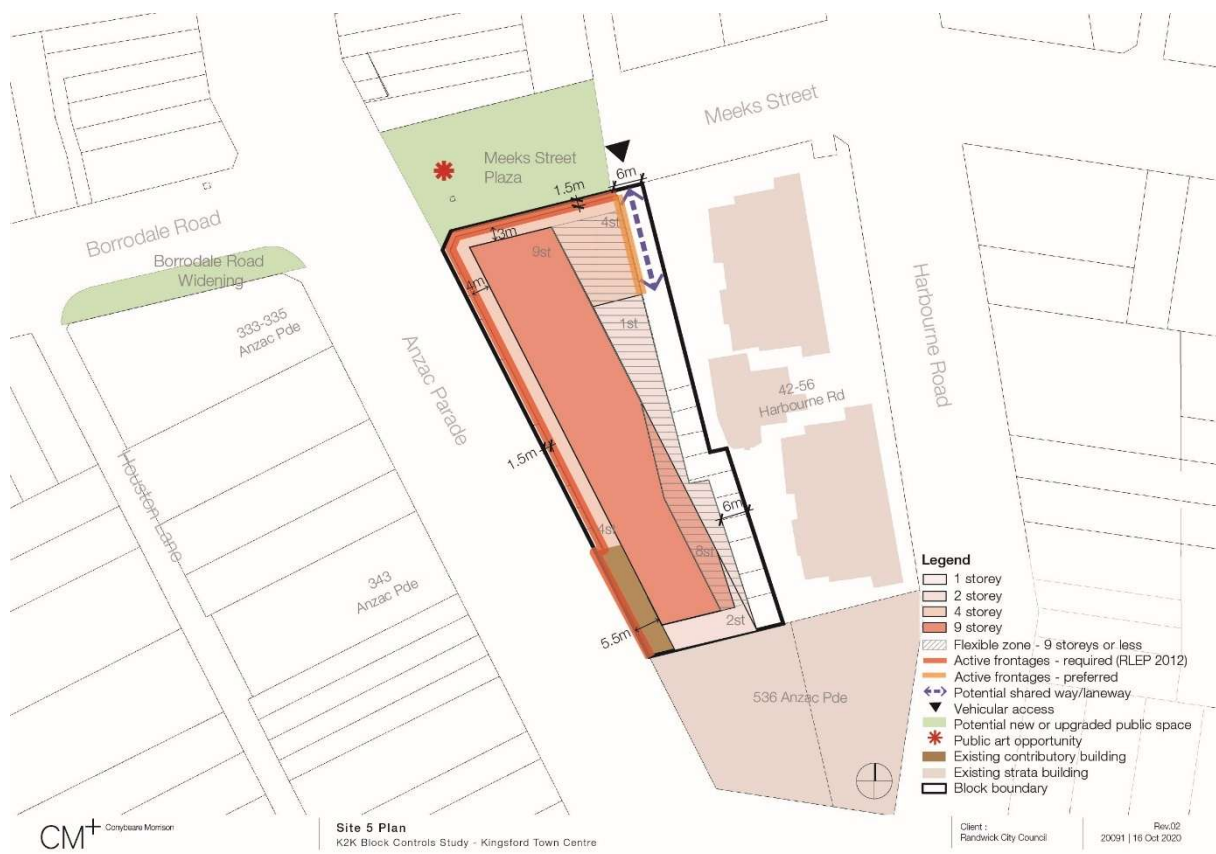
The preferred outcome for the block is a single building envelope that has a strong visual connection to the Rainbow Street strategic node site across the road to the south and the wider Kingsford Junction Precinct. A pedestrian link on the northern boundary will provide a connection between Forsyth Street and Harbourn Road. A flexible zone is provided on the northern side to enable built form to be suitably distributed across the site.

The block will present as 9 storeys to the Rainbow Street, Harbourn Road and Forsyth Street frontages, stepping down to provide an appropriate scale transition to the pedestrian link to the north. The building will have active street frontages to contribute to the thriving retail, commercial and dining character of the Kingsford town centre. The block will maintain a fine grain urban structure through appropriate modulation and articulation to ensure it has a cohesive relationship with the smaller shop fronts along the southern part of Kingsford town centre.

Figure 16b: Block 4



Block 5
Figure 17a: Block 5



Desired future character

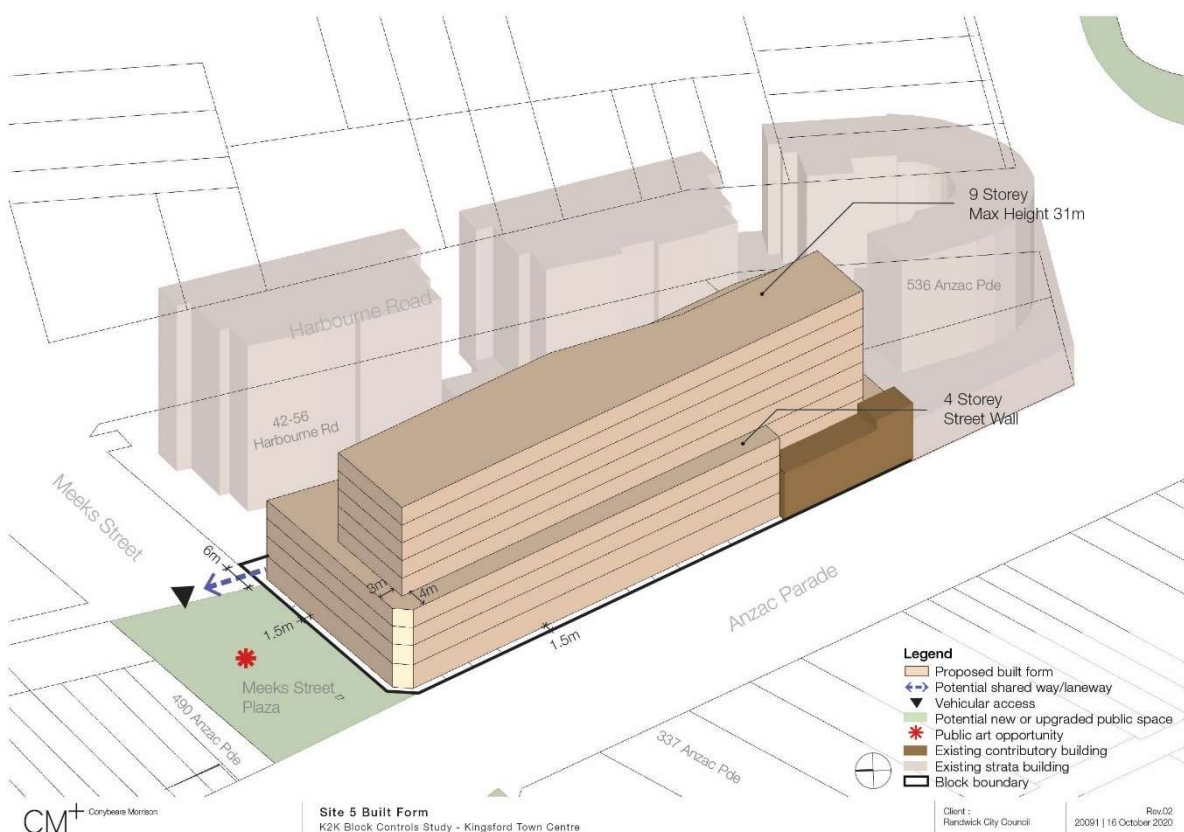
The block is presently occupied by a row of relatively uniform fine grain 2 storey shop fronts offering a mix of retail, restaurants and café services. A contributory building is located on the south western corner, abutting the Churchills development to the south. To the north is Meeks Street Plaza which is a key focal point within the town centre and subject to an upgrade to improve its useability and amenity.

The preferred outcome for the block is an amalgamated mid-rise building oriented to Anzac Parade with façade articulation to complement the existing fine grain streetscape. Built form is to step down to four storeys fronting Meeks St Plaza. A flexible zone is provided along the eastern boundary to enable distribution of density across the site. Vehicle access is to be provided at the rear through a shareway/laneway off Meeks Street. A 1.5m ground floor setback is to be provided off Meeks Street Plaza to improve the public domain.

The development fronting Anzac Parade is to complement the heritage streetscape character of the Kingsford town centre and reinforce the street edge with a 4 storey street wall. New development must incorporate proportions and rhythm that is sympathetic to the contributory building. A 4m upper level setback to Anzac Parade and 5.5m upper level setback to the Churchills development will reinforce the historic visual appearance of the contributory building, allowing its primary building form and articulation to be retained. The southern and eastern interfaces between this block and the adjoining development should be carefully designed and resolved to coordinate with existing window openings to ensure adequate amenity and separation is achieved. A flexible zone provides for built form to be suitably distributed to ensure compliance with the ADG.

Active frontages along Anzac Parade and Meeks Street adjacent to the plaza will provide a positive interface with street life and reinforce Kingsford town centre as a retail and dining destination.

Figure 17b: Block 5



Block 6

Figure 18a: Block 6



Desired future character

The block comprises a row of contributory building shop fronts containing mostly restaurants, cafes and some convenience retail. The block is located in the heart of the dining strip in the Kingsford town centre and has a thriving night-time economy. Meeks Street Plaza and Council car park adjoins the block to the south and east. A contemporary mixed-use development is situated to the north, at 458 Anzac Parade.

The preferred outcome for the block is a mid-rise building form of 9 storeys stepped down to 5 storeys along the Middle Lane frontage to provide a mews style development that takes advantage of Middle Lane at the rear which facilitates shared access. A 2m ground floor setback is to be provided off Middle Lane. A flexible zone is included across the middle and northern edge to enable built form to be designed to transition across the block and address existing development to the north at 458 Anzac Parade.

Sensitive design will integrate contemporary elements with historic fabric. A 5.5m upper level setback above the contributory buildings will help to ensure that historic form, proportions, and articulation is retained in the redevelopment of the block.

Built form on the southern and eastern edge must actively address Meeks Street Plaza and car park with design elements that provide a high-quality presentation, passive surveillance and visual connections between the building and public realm.

Future development opportunities that support the dining destination character of the precinct are encouraged.

Figure 18b: Block 6



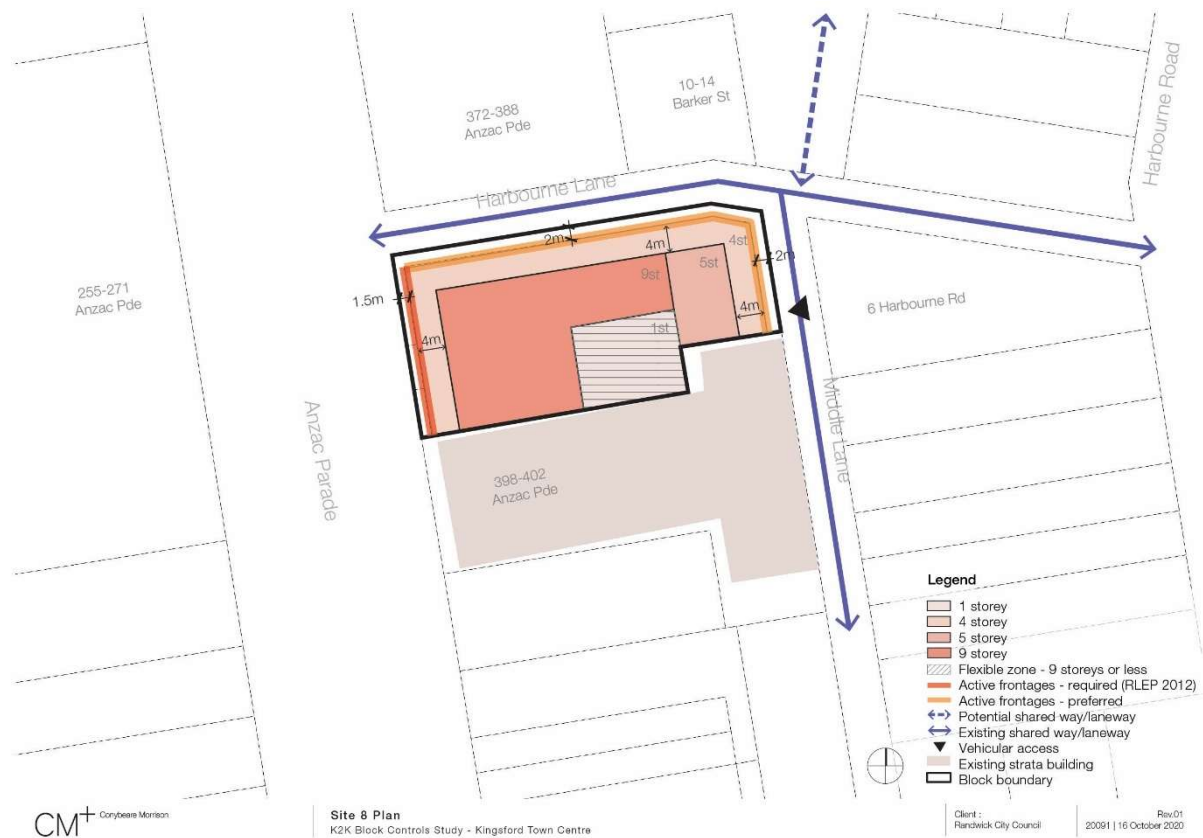
Block 7

Note

Refer to Kingsford midtown provisions (K5, K6 and K7)

Block 8

Figure 19a: Block 8



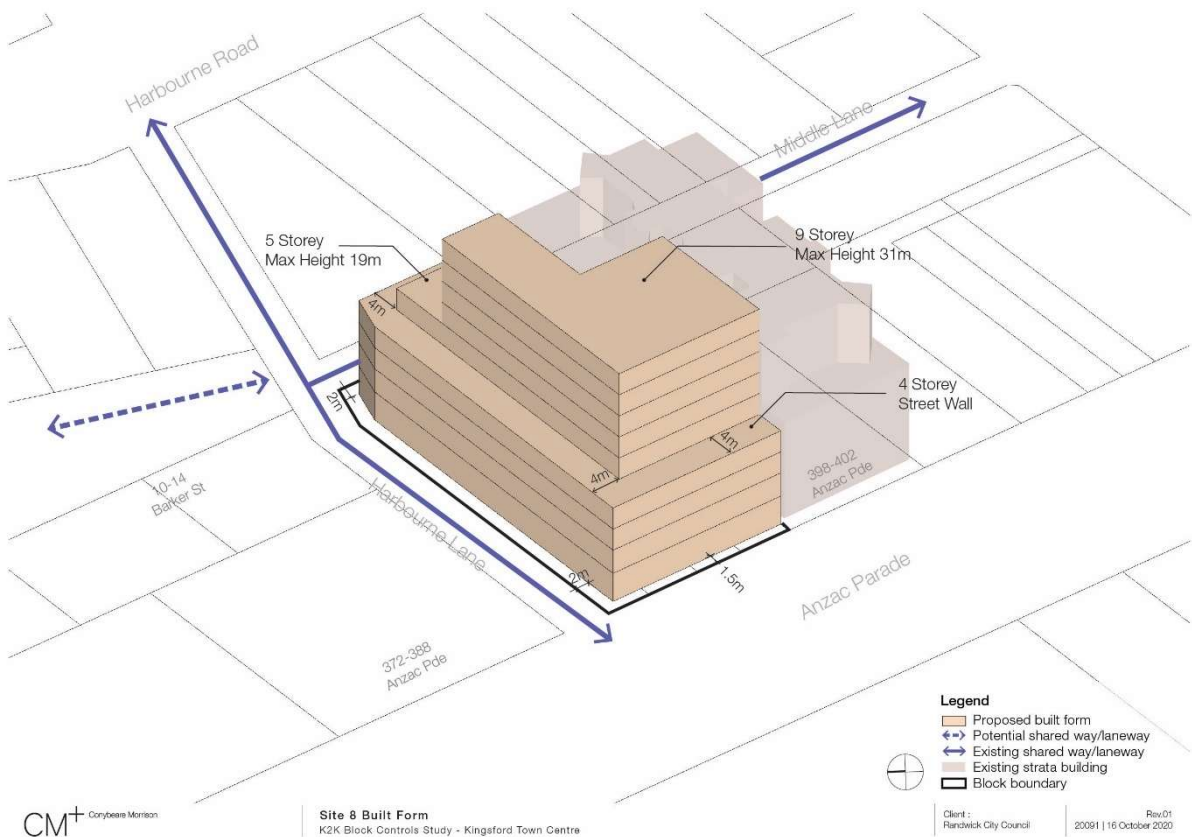
Desired future character

The compact block is located at the corner of Anzac Parade and Harbourn Lane. It is presently occupied by three single storey shop fronts along Anzac Parade and an automotive services business fronting Harbourn Lane. To the south at 398-402 Anzac Parade is a tall multi storey mixed use development.

The preferred built form outcome for the site is for a building aligned to Anzac Parade, Middle Lane and Harbourn Lane. Development is to maintain the prevalent mid-rise building typology desired for the town centre, stepping down to 5 storeys at the rear towards Middle Lane. Vertical built form and roof top elements would emphasise the corner of Anzac Parade and Harbourn Lane. A flexible zone is provided on the southern side which enables creation of either an internal courtyard or common space for residents or an alternative design response that meets the ADG.

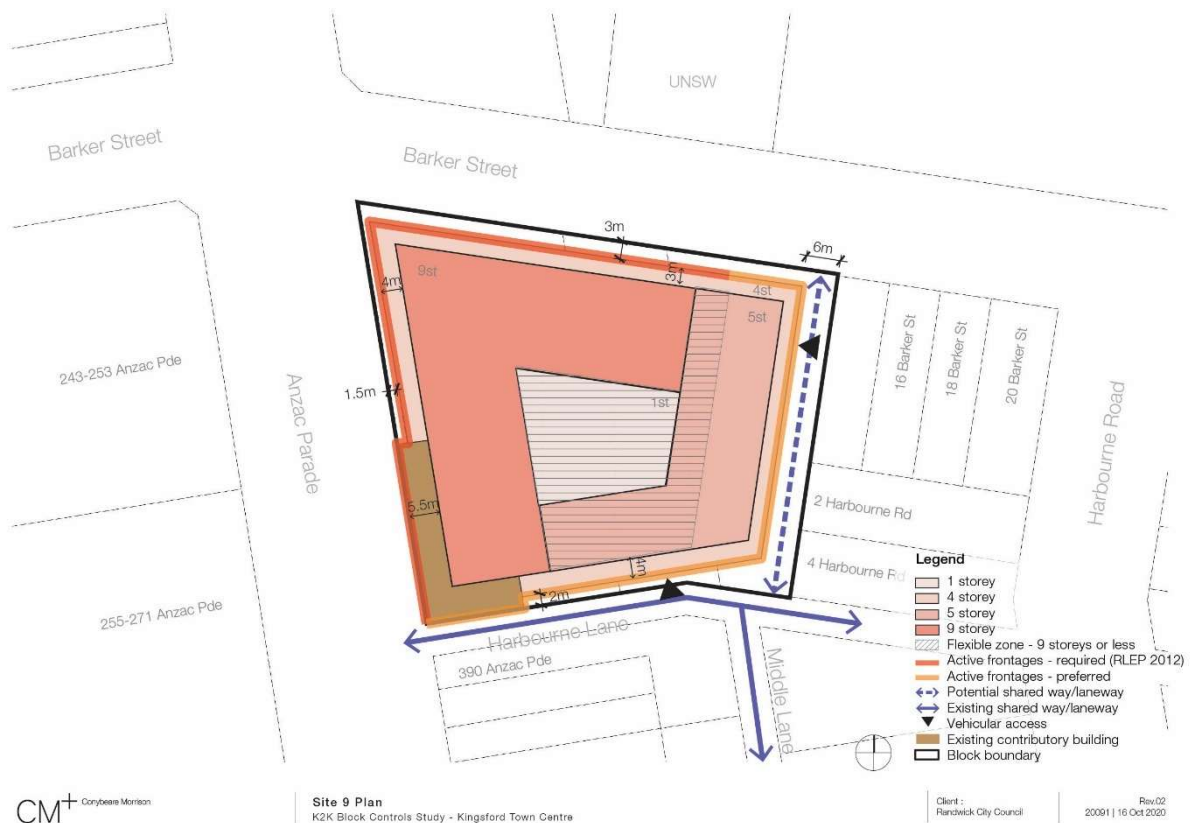
Active frontages along Anzac Parade and Harbourn Lane are required to contribute to the vitality of the streetscape and the wider town centre. A setback of 2m on the Harbourn Lane frontage will create opportunities for landscaping, footpath dining and activation of this laneway.

Figure 19b: Block 8



Block 9

Figure 20a: Block 9



Desired Future Character

Block 9 is located on the corner of Anzac Parade and Barker Street and is currently occupied by a McDonald's Restaurant and BP Service Station. The site is opposite UNSW Campus and experiences high pedestrian and vehicular traffic volumes. The site has excellent access via its 2 main street frontages and Harbourne Lane.

The preferred development outcome for the block is to enable adequate building setback of 3m on the northern boundary to Barker Street to provide for future widening of traffic lanes as well as pedestrian movements. This would provide for a significant improvement to the public domain at this key intersection.

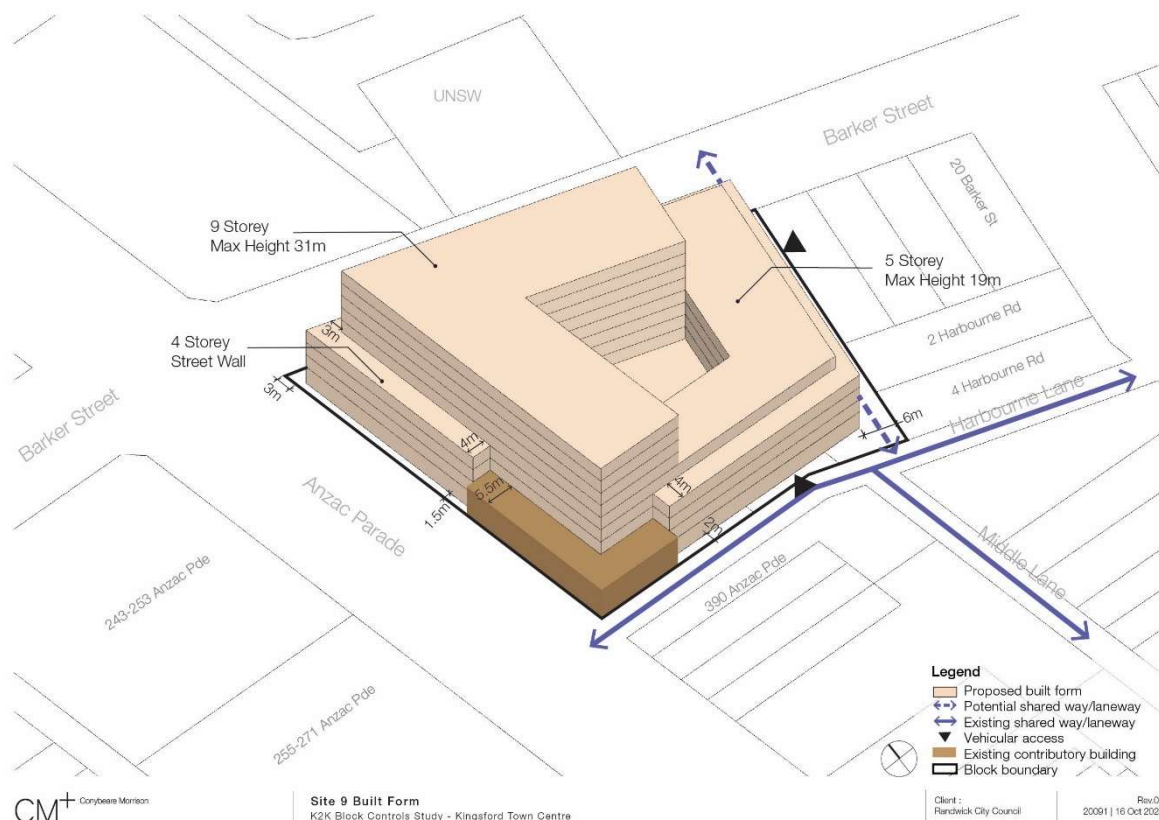
Future development on this site should focus the higher 9 storey component onto Anzac Parade and Barker Street behind a 4 storey street wall. The development requires a design response that is sensitive to the historic form and fabric of the existing contributory building. Upper level setbacks of 4m are required around the perimeter of the block. A flexible zone is included through the centre and eastern side to enable built form to be suitably distributed across the site in response to ADG requirements for separation and amenity.

A north-south shareway is to be provided connecting Barker Street with Harbourne Lane.

The fine grain proportions of existing shop fronts will be interpreted through a well articulated built form particularly on the Anzac Parade frontage to contribute towards a cohesive streetscape within the Kingsford town centre.

Continuous active frontages are to be provided along Anzac Parade through appropriate location of uses such as shops, cafes, and restaurants, to facilitate a visual connection between the building and public realm and support a thriving economy.

Figure 20b: Block 9



Block 10

Figure 21a: Block 10



Desired future character

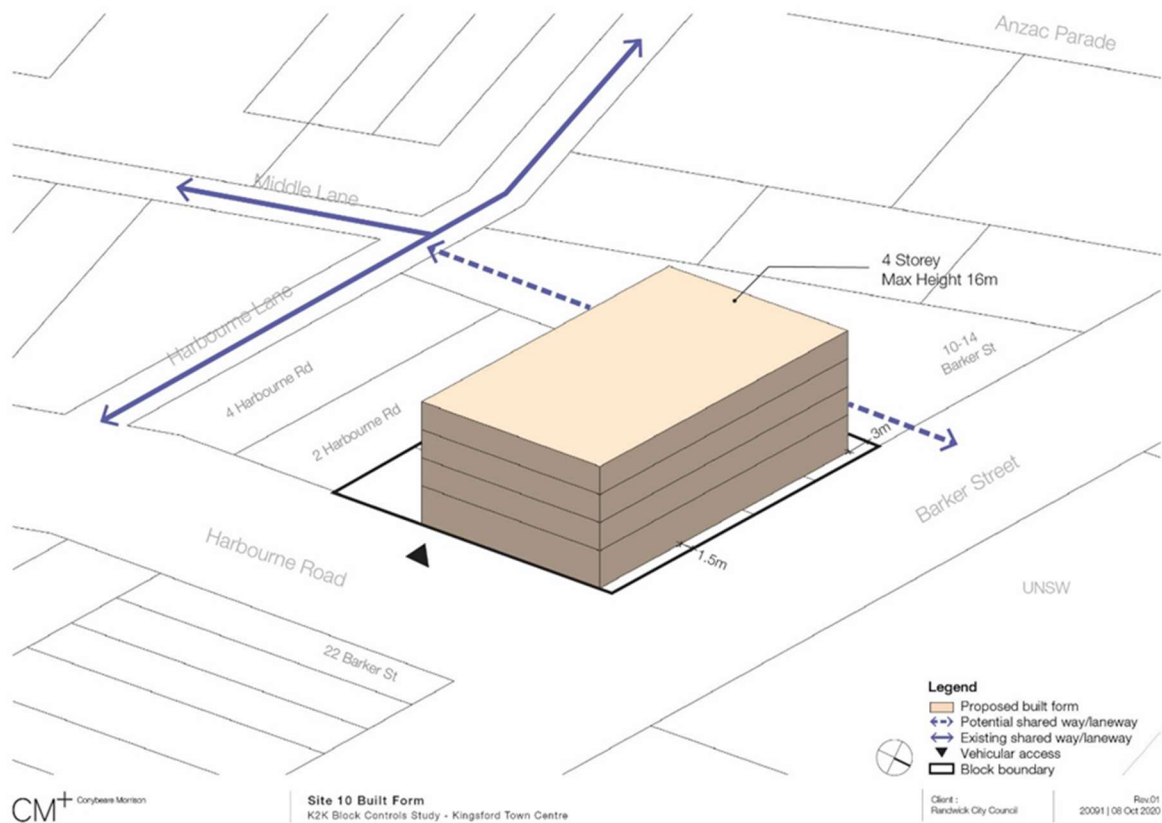
Block 10 is located on the corner of Barker Street and Harbourne Road opposite the UNSW Campus and adjoining the McDonald's Restaurant on the western boundary. The block comprises 3 lots and contains two single storey dwellings and a 4 storey residential flat building with 6 units. The site is opposite UNSW Campus and experiences high pedestrian and vehicular traffic volumes. The site provides good access via its 2 main street frontages and vehicular access from Harbourne Road.

Harbourne Road is residential in nature and has varied building heights ranging from 1 to 4 storeys, with consistent front setbacks, boundary fences and architecture styles. Adjoining the block to the south is a residential flat building at No. 2 Harbourne Road and a single storey dwelling at No.4 Harbourne Road.

The preferred development outcome for the block is for a four storey building fronting Barker Street that provides for active ground floor retail and business activities that wrap around three street frontages. A 1.5m front building setback would facilitate a continuous footpath widening extending from this site to the intersection with Anzac Parade. A 3m building setback is to be provided along the western side.

Development on the site is to respect the lower scale residential development to the south by providing a minimum 6m rear building from the boundary. A flexible zone is included on the southern side to enable built form to be distributed and transitioned across the site towards the residential area fronting Harbourne Road.

Figure 21b: Block 10



Blocks 11 and 12

Note

Refer to Kingsford Junction Block Provisions