

DEVELOPMENT  
CONTROL PLAN  
*Randwick Junction*



*Council Approval: 2 February 1999*  
*Effective Date: 18 February 1999*

RANDWICK  CITY COUNCIL

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## **Part 1**

# **GENERAL INFORMATION**

## 1.1 INTRODUCTION

This Development Control Plan (DCP) has been prepared in accordance with the provisions of the *Environmental Planning and Assessment Act 1979* and the *Environmental Planning and Assessment Regulation 1994*.

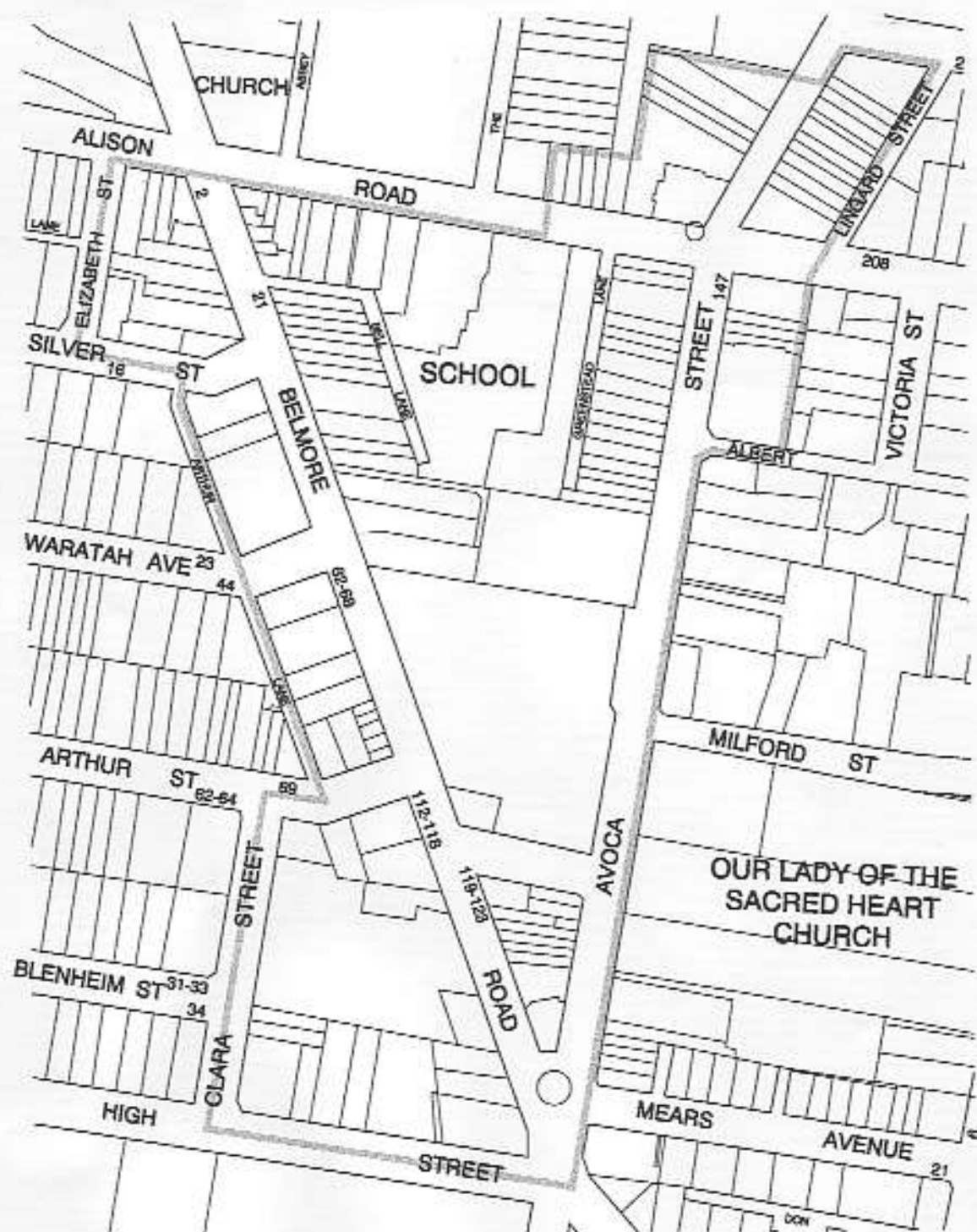
This DCP applies to all land within the Randwick Junction Heritage Conservation Area, as identified in Randwick Local Environmental Plan (LEP) 1998, refer to Figure 1. It supplements Randwick LEP 1998 and contains detailed development controls, standards and guidelines for all development within the Randwick Junction Heritage Conservation Area.

## 1.2 AIMS

This Development Control Plan aims to:

- implement the aims and objectives of Randwick Local Environmental Plan 1998;
- implement the findings of studies that relate to Randwick Junction (See References);
- provide clear and concise guidelines and design standards to regulate development in Randwick Junction;
- encourage environmental and architectural design excellence and civic accountability;
- encourage the use of energy efficient design techniques;
- minimise the adverse impacts of development on the environment of Randwick Junction on resident and user amenity and the existing environment;
- encourage development which is compatible with the significant heritage character in Randwick Junction;
- promote development that enhances the existing urban character and provides linkages between past and future development, both in terms of scale and architectural expression;
- produce an environment which is both dynamic and vital, whilst maintaining an "urban village" character for Randwick Junction; and
- manage the future orderly development of Randwick Junction in a manner which meets community expectations and which maintains the overall commercial viability of the centre.

Figure 1 Map of Randwick Junction showing area to which this Development Control Plan applies.



Area to which this plan applies

### 1.3 DESIRED FUTURE CHARACTER

#### Objectives

- Maintain and enhance the positive attributes of the existing built environment within Randwick Junction;
- Ensure that new development compliments and contributes positively to the built environment of Randwick Junction;
- Promote environmental design standards that respect the existing character of Randwick Junction;
- Encourage the provision of a variety of residential and commercial accommodation in Randwick Junction; and
- Encourage a diverse range of commercial activities that enhances the vitality of Randwick Junction as a desirable business location.

#### Explanation

Several buildings and places within Randwick Junction have significant heritage and architectural qualities that make the townscape sensitive to changes in the built environment. The location of Randwick Junction along a prominent ridgeline means that new development within the Heritage Conservation Area has the potential to affect a large visual catchment.

When considering a development application, Council will assess the effect of the proposal on the aesthetic qualities of the townscape and the heritage significance of the area.

#### Architectural Character in Randwick Junction

Randwick Junction's architecture is characterised by:

- buildings in Belmore Road and Alison Road predominantly built to the street alignment;
- commercial buildings in Avoca Street built to the street alignment with existing and former (often Victorian )



*Existing Streetscape in Belmore Road showing consistent scale of development, buildings of heritage value from a number of periods in Randwick's history as well as the impact of disruptive influence of above-awning signage and poorer quality of more recent infill development.*



residential buildings set back from the street. These buildings typically have fences that reinforce the street alignment on the front boundary;

- mixed commercial and residential usage within the same building;
- rich architectural detail;
- parapet height generally between 9m and 12m from ground level;
- embellishment at the parapet / skyline; and
- polychromatic brickwork and / or strongly contrasting materials (eg. face brick and roughcast render) or a rendered and painted facade.

These characteristic elements of Randwick Junction should be used in the design and construction of new development to reinforce and promote the special qualities of the centre.

### Civic Improvement

The Urban Design Study of Randwick and "The Spot" Shopping Centres 1994, by Knox and Tanner Pty. Ltd. makes several recommendations for civic improvement in and around Randwick Junction, noting that:

"it is important that future urban design projects acknowledge and enhance the special qualities of the place. At all levels, from the conceptual to the detail understanding of *genius loci* image and character should influence decisions"

The study was adopted by Council, and its recommendations form the basis of Council's capital works program in the Section 94 Plan for Randwick Junction. The capital works program includes provision for a safer pedestrian environment, landscaping and increased parking.



This recent development in Alison Road adopts the desired scale, form and proportions envisaged by this DCP. Note the use of wooden frame windows on the shopfronts and the detailing on the render to add visual interest.

## 1.4 HOW TO USE THIS PLAN

This DCP operates under the provisions of Randwick Local Environmental Plan (LEP) 1998, which contains the primary land use controls such as the zoning of particular land. You should refer to the LEP to establish the types of uses permissible on individual land parcels affected by this DCP, prior to formulating any development proposal.

Each set of design considerations and the various functional requirements under this DCP have been placed into specific sections such as Existing Character; Site Planning and Building Design; and Outdoor Advertising and Restaurant Seating.

The objectives for each of these sections are stated along with a brief explanation of their purpose and intent.

Any relevant statutory controls from Randwick LEP 1998 are identified, however intending applicants should still consult with the LEP for detailed and up-to-date statutory controls. These are further clarified and expanded upon by specific DCP Controls (DCP Controls), which are stated within the accompanying boxes.

## 1.5 DEVELOPMENT APPLICATIONS

All Development Applications within the Randwick Junction Heritage Conservation Area will be assessed and determined under the provisions of the *Environmental Planning and Assessment Act, 1979*. Detailed requirements are to be considered in Section 79C of the Act. Compliance with the numerical standards contained within this DCP does not mean that an application will be approved. All applications will be assessed on their merits and how well they achieve the objectives and performance requirements.

Any departures from the requirements contained within this DCP will need to be fully justified, with specific reference to the objectives of the control proposed to be varied.

Intending applicants are advised to consult the Randwick Development Application Guide prior to preparing their proposals for lodgement with Council.



## **Part 2**

# **EXISTING CHARACTER AND QUALITY**

## 2.1 VISUAL CHARACTER

Randwick Junction is situated on a ridge line that is highly visible from many vantage points within the local government area, such as South Coogee, Clovelly and Kingsford. In turn, it is possible to see vistas to the east (towards Gordon's Bay and Coogee), west (towards Kensington and Surry Hills) and south (towards Kingsford and Maroubra) from Randwick Junction through these existing view corridors.

The existence of these view corridors and the other significant viewsheds mentioned below should be addressed in the site analysis for new developments. This may necessitate adjustments in the built form and site layout in order to accommodate them.

The dominant view corridors towards and out of Randwick Junction are to the north east and the south along Avoca Street, to the east and west from Alison Road and to the south east (towards South Coogee) along the axis of Belmore Road. Minor internal vistas also occur along Alison Road, Avoca Street and Belmore Road. New development should not intrude into these view corridors and vistas.

There are several landmarks and prominent buildings within the precinct which are visible from adjoining areas, some of which even afford distant views of the centre. These include the statue of Captain Cook at the

intersection of Avoca Street and Belmore Road, the castellated tower of the former Star and Garter Inn at the intersection of Avoca Street and Belmore Road, the old Post Office at the corner of Belmore Road Short Street and the commercial building at 139-145 Avoca Street. Views to or from these buildings and landmarks contribute strongly to the visual character of Randwick Junction and should not be obstructed by new development.

It should be noted that several important landmarks are located just outside the boundaries of Randwick Junction Heritage Conservation Area (HCA) in the adjacent HCA'S. These include the Sacred Heart Church, St Judes Church (and associated buildings) the Town Hall (all in Avoca Street), High Cross Reserve, the Royal Hotel and The Prince of Wales Hospital.

The townscape within Randwick Junction shares an important visual relationship with these landmarks as well as with the adjacent Heritage Conservation Areas of High Cross, Sacred Heart and St Judes. New development should be designed to fit within this wider townscape and should not disrupt the existing visual relationships both within and without the precinct.

*The Spire of the Sacred Heart Church is an important landmark within the local context, as seen here from Avoca Street, looking to the south.*



Important views to consider include:

- views across Alison road to the Presbyterian Church, Alison Park, Victorian terraces on The Avenue and Abbey Street, The Council Administration Building, St Judes Church and Cemetery;
- views along Avoca Street to the Town Hall, St Judes Church, Sacred Heart Church, High Cross Reserve, The Prince of Wales Hospital and the Royal Hotel; and
- views along Belmore Road to High Cross Reserve, Perouse Road and the Royal Hotel.

## 2.2 URBAN FORM

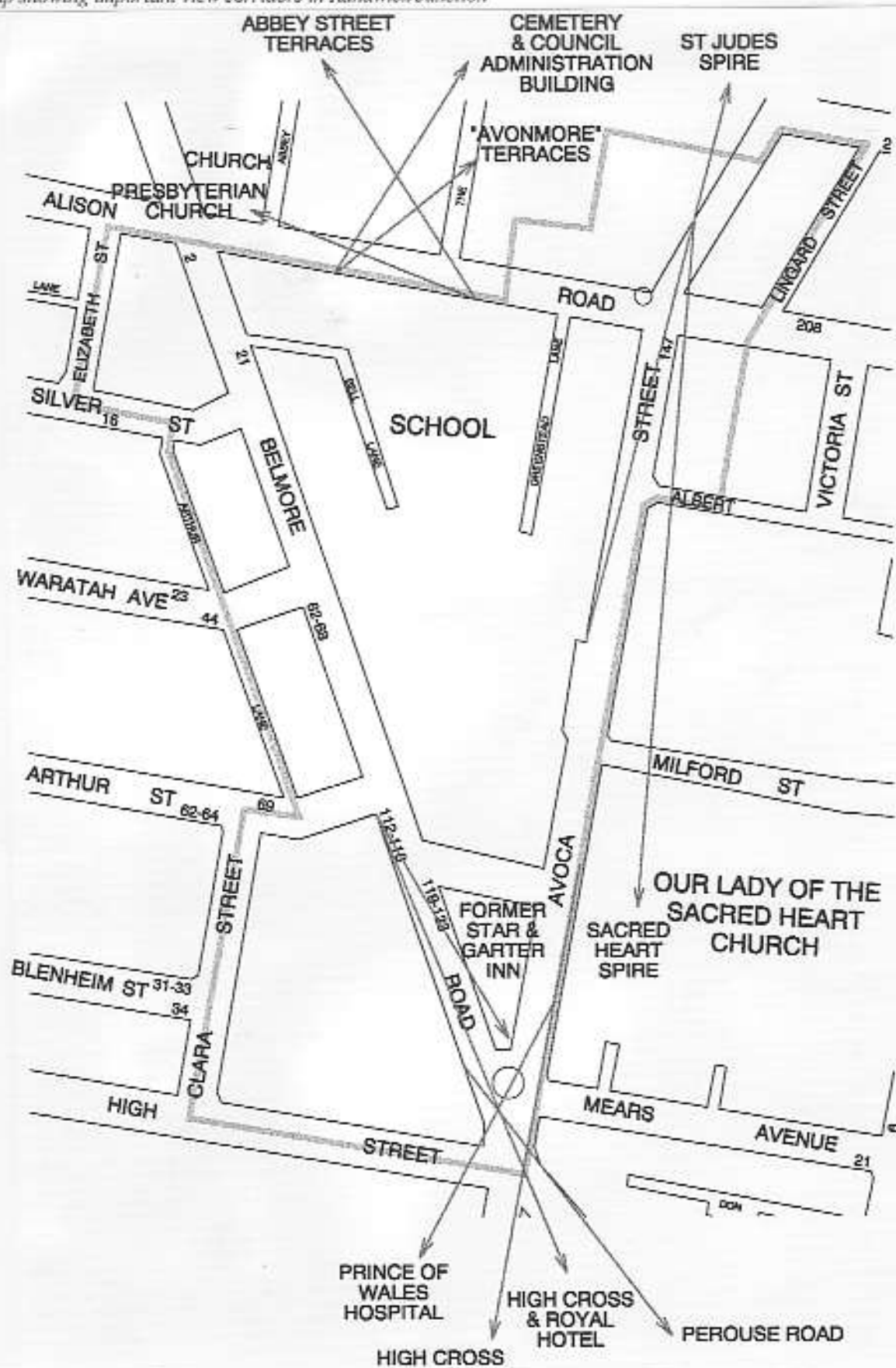
Randwick Junction is essentially an old style strip shopping centre. The buildings which line the two major streets typically contain ground floor retail space with two to three storeys of residential or office accommodation above. This has produced a human-scaled streetscape environment, which gives a comfortable scale of enclosure and which is highly permeable for pedestrian users.

The urban form of Randwick Junction has evolved over a considerable period of time. It is characterised by two major periods of development, occurring around the turn of the century and between the two World Wars.



*View of Belmore Road south of Arthur Street, showing consistent 2-3 storey scale of development and characteristic enclosure of streetscape by buildings built to front and side boundaries and continuous awning cover.*

Map showing important view corridors in Randwick Junction



Area to which this plan applies

A substantial proportion of the building stock from each period of development still remains in a relatively intact form today. This is reflected in the identification of Randwick Junction as an Heritage Conservation Area under Randwick LEP 1998.

The early street pattern has largely remained unchanged, with the significant changes occurring mainly along Belmore Road, where the original suburban cottages of the mid-nineteenth century were gradually replaced by shops built to the street frontage and the lots subdivided into narrow commercial frontages. Out of these original suburban dwellings, only Sandgate (subject to a Permanent Conservation Order) remains and maintains the original setback (although the Randwick Plaza development has encroached on its frontage).



*View of Avoca Street looking south towards the intersection with Alison Road. Note the consistency of parapet heights and window placements and the high degree of enclosure of the streetscape. The increased height of the parapet on the corner building reinforces the corner.*



## 2.3 STATEMENT OF SIGNIFICANCE

The Randwick Junction Heritage Conservation Area is centred on the Randwick Junction commercial centre. It is generally bounded by Belmore Road, Alison Road and Avoca Street, Randwick. Most of the land within the conservation area is zoned General Business 3A, however it does contain significant pockets of land zoned Special Uses and Residential 2C.

It is significant to note that it is the only conservation area within the City of Randwick that is focussed on a commercial centre. It retains a coherent streetscape character of nineteenth and early twentieth century buildings. Within the conservation area there are two distinct groupings of commercial buildings. These are the Belmore Road and the "Coach and Horses" grouping (centred around the intersection of Alison Road and Avoca Street).

### 1. Historic Significance

Randwick Junction has been the centre of commercial activity in Randwick since the establishment of the village in the mid-nineteenth century. The buildings in the conservation area provide physical evidence of the process of growth and development of Randwick as a commercial centre. The conservation area is at the intersection of three roads that have been the principal routes for travel between Randwick and other parts of Sydney since the establishment of the suburb. The first Randwick - Sydney horse omnibus and the first mail service were established on the site of the Coach and Horses Hotel in 1859, reflecting a strong relationship between Randwick Junction and early transport and communications in the district.

Many of the important sites in the early development of the commercial area were at the street intersections. The former Star and Garter Inn, constructed circa 1859, was one of the earliest hotels in Randwick. No. 119 Belmore Road, at the corner of Short Street, was the site of the post office from 1878 to

1897.

The most rapid period of growth began after the introduction of steam trams in 1881. The 1880's were a period of large scale subdivision in Randwick. In the Federation and Inter War periods development of the commercial centre continued. There was considerable expansion on the western side of Belmore Road. Earlier, less intense residential uses, such as "Sandgate" at 128 Belmore Road, were displaced.

The foundation stone for Randwick Post Office in 1897, on the northwest corner of Alison Road and Avoca Street. This building provides historical evidence of the importance of the conservation area as a centre of communication and reflects the connection to government and institutions within Randwick.

### 2. Aesthetic Significance

The conservation area is a good and generally intact example of a traditional commercial "strip" (linear) style centre. Buildings are typically two to three storeys and are generally built to the street alignment, for the full width of the allotment. The urban spaces formed by the buildings impart a strong linear character, particularly along Belmore Road. There are many good examples of buildings from the Victorian, Federation and Inter-War periods.

In the Coach and Horses grouping the Victorian Italianate style is dominant, interspersed with other later styles such as Federation Freestyle. There are significant groups of these buildings on the south-west corner of Avoca Street and Alison Road, as well as on the east side of Avoca Street, north of Alison Road. There are excellent examples of Victorian Italianate commercial and residential buildings on Alison Road, between Avoca Street and Belmore Road as well as three outstanding Victorian Italianate residences on Avoca Street, adjacent to Marcellin College.

The single most striking building within the conservation area is the former Star and Garter Inn, at the corner of Avoca Street and Belmore



Road, notable for its distinctive castellated sandstone tower and the adjacent statue of Captain James Cook. The pairing of the Coach and Horses Hotel and the former Post Office, located on diagonally opposite corners of the intersection of Alison Road and Avoca Street is also prominent.

### 3. Social Significance

The conservation area continues as Randwick's main commercial centre, developing around the earliest hotels at Randwick, namely the former Star and Garter Inn and the Coach and Horses Hotel. The Victorian, Federation and Inter War buildings provide a sense of historical continuity throughout the centre and the streetscape character of the conservation area are well recognised throughout the community. In 1923, the Catholic Church acquired the Brisbane Villa Estate on Alison Road for a monastery. This site later became the Marcellin College, an important and enduring centre for education within the local community.

When considered further in the context of the two adjacent conservation areas of St Judes and High Cross, with their significant administrative, cultural and institutional roles, Randwick Junction may be seen as the focal point of the city, as many of the enduring symbols of Randwick's development are located either within or immediately adjacent to the conservation area. Important community services such as mail services and government savings bank (initially operating from the post office), as well as educational and commercial activities have been centred in and around Randwick Junction for as long as the suburb has been established.



*The old Randwick Post Office occupies a prominent corner site on the corner of Alison Road and Avoca Street. This corner has long been the centre of community activity in Randwick, dating back to the establishment of the Coach and Horses Hotel and Veness' store.*

#### 4. Relevant Historical Themes

- *Government and Institutions*
- *Industry and Commerce*
- *Speculation and Promotion*
- *Promotion of Culture- Religion and Education*
- *Recreation, Entertainment and Leisure*
- *Transport and Communication*

identified on the map entitled "Randwick Heritage and Streetscape Controls" in this section. A listing of the individual properties and their streetscape ratings is included within Schedule 2 of Appendix 1 in this document.

From *Randwick Heritage Study- Specialist Report Architecture and Townscape: Table of Themes*; Randwick Municipal Council 1989.

#### 5. Other Contributing Elements

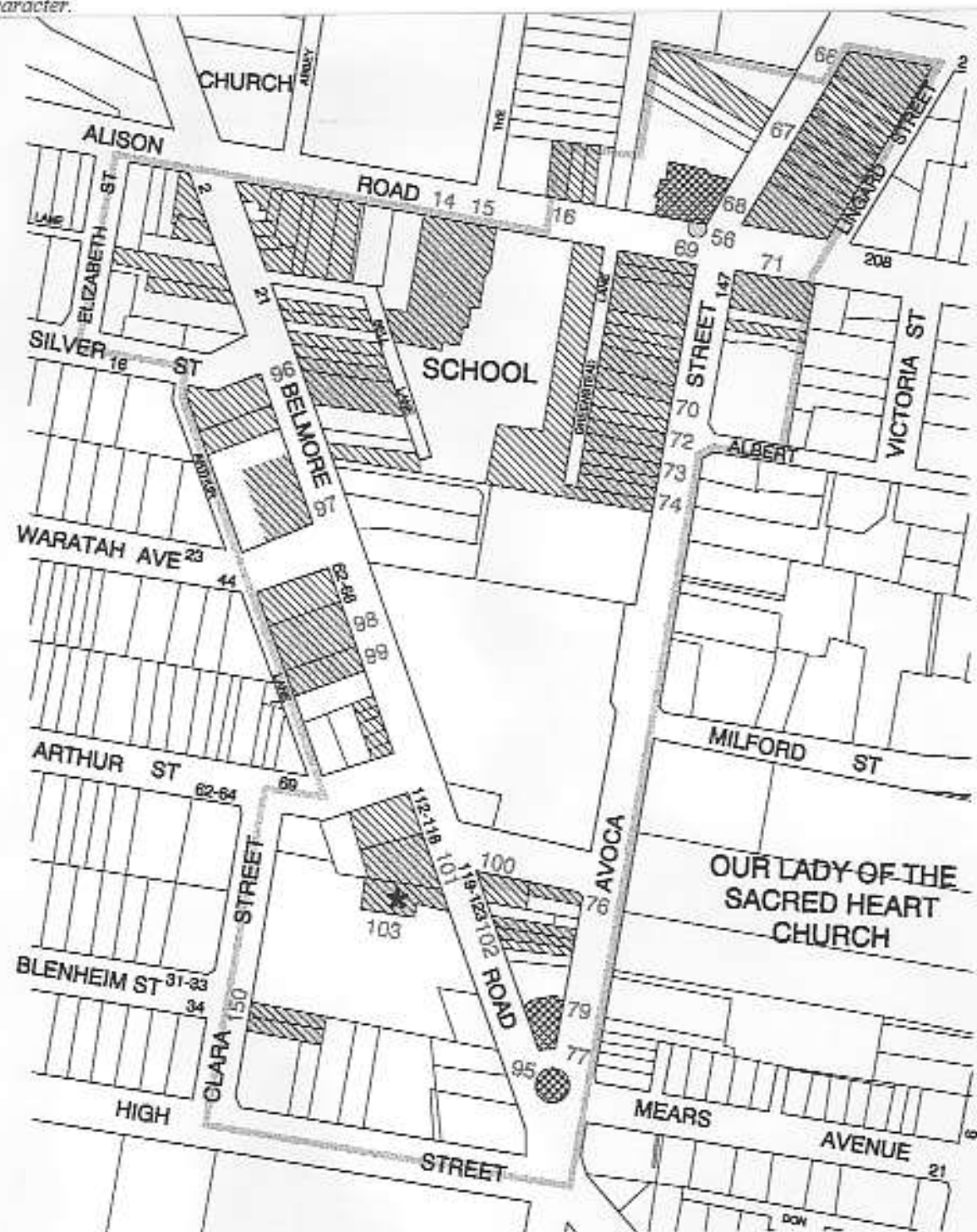
Several building types present within the Heritage Conservation Area enhance the aesthetic qualities and distinctive character of Randwick Junction. They also tell us much about the development history of the area and many have heritage significance even though they are not identified as heritage items. For example, early twentieth century flats are reminders of the relationship between the tram (efficient public transport) and extraordinary population growth in Randwick both before and after World War I.




In addition to the buildings listed in the Randwick LEP 1998 as heritage items, a significant proportion of the remaining buildings within the Randwick Junction Conservation Area also contribute to its overall significance. These buildings were identified in the Knox and Tanner *Urban Design Study of Randwick and "The Spot" Shopping Centres, Volume 2*.

It is desirable to conserve the buildings identified by Knox and Tanner as "critically important," "important," and "contributing" to the streetscape and these listings should be considered when making determinations under clause 43 of Randwick LEP 1998 with regard to heritage matters on proposals within the Randwick Junction Heritage Conservation Area.

These buildings, along with those heritage items identified in Randwick LEP 1998 are

Map showing Heritage Controls within Randwick Junction, including buildings which contribute to streetscape character.



-  HERITAGE CONSERVATION AREA
-  HERITAGE ITEM (RANDWICK LEP 1998)
-  BUILDINGS WHICH CONTRIBUTE TO STREETScape CHARACTER (URBAN DESIGN STUDY OF RANDWICK & 'THE SPOT' SHOPPING CENTRES KNOX & TANNER PTY LTD, 1994)

-  PERMANENT CONSERVATION ORDER (HERITAGE ACT)
-  REGISTER OF THE NATIONAL ESTATE (AUSTRALIAN HERITAGE COMMISSION)

## 2.4 ARCHITECTURAL STYLES

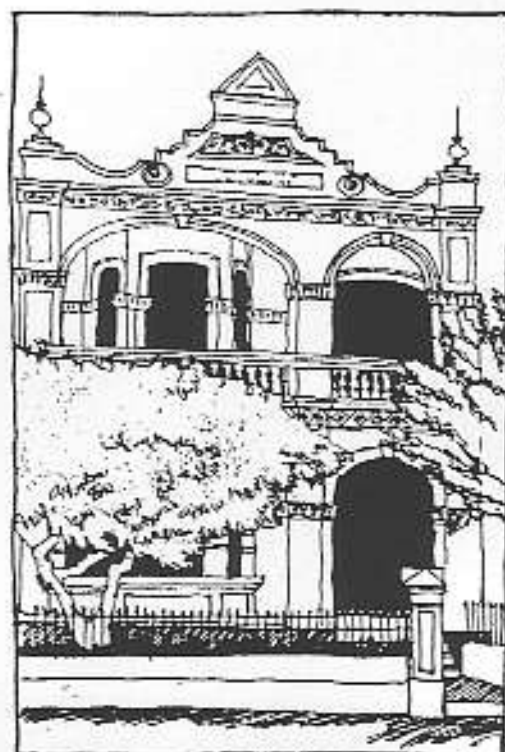
### Victorian

Randwick Junction's nineteenth century buildings were at one time predominantly residential and have since been adapted for a range of uses including professional office suites. They are constructed of load-bearing masonry and, usually, rendered and painted, with deep recessed verandahs, iron lace and intricate classical moulding and detail. An exception is the stylistically eclectic former Star and Garter Inn. This complex includes an observation tower and residence constructed of sandstone.

Some late Victorian buildings, such as 167-171 Alison Road, are constructed of face brickwork.

### Federation / Edwardian

The Post Office, an important local institutional building, is also constructed of face brickwork with contrasting sandstone elements, and nearby, at 110-116 Avoca Street, a row of early twentieth century shop-fronts relies on the use of contrasting brick, ceramic tile and stucco for architectural effect.



*Torquay at 150 Avoca Street, is one of an outstanding group of three Italianate houses built in 1906. Although a late example of a Victorian Italianate Terrace, it incorporates many of the features of the style, such as the use of a richly decorated and elaborately formed parapet, segmented bay windows, stilted round arches and smooth stucco finish. There are three similar houses at 126, 128 and 130 Alison Road.*

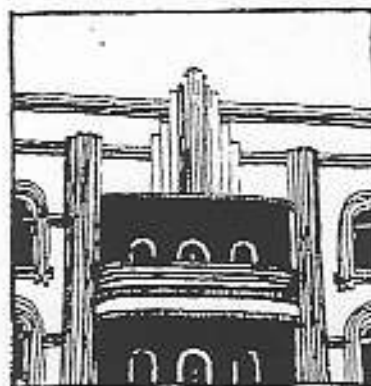


*Row of Early 20th Century Shops at 110-116 Avoca Street, exhibiting Federation Free-Style exhibiting such characteristics as substantial surface relief and variation in texture (ie. face brick, roughcast render, coloured glazed tiles, painted face brickwork, and decoratively patterned render on the verandah. Diocletian window motifs and a classical balustrade on the parapet are also in evidence.*



## 1920-1940

Randwick's Inter-war Art Deco architecture is characterised by rich detail, polychromatic brickwork and decorative vertical piers and fins, often extending above the line of the parapet to give vertical emphasis. Recessed shadowy balconies with solid, sometimes curvilinear, balustrades also add interest to the facade. The building on the corner of Belmore Road and Waratah Avenue, with its exuberant brickwork and architectural detailing, is an excellent example of local Art Deco architecture.



*Detail of the Art Deco Building on the corner of Belmore Road and Waratah Street showing surface relief, facade modulation and skyscraper parapet treatment.*

## Post World War II (1940 to the present)

Several buildings constructed since 1940 impact with varied success on the character of Randwick Junction. These include two major shopping centres, the Gemini Motel, the Marist Brothers school site, and other smaller developments.

Perhaps the most significant and visually intrusive post war development in Randwick Junction occurred during the 1970's when a few eight to ten storey buildings were constructed. These buildings are two to three times the height of neighbouring buildings and are inconsistent with the predominant scale of development in Randwick Junction and nearby residential areas.

For more information regarding architectural styles, see the sections relating to Victorian Filigree, Federation Filigree, Federation Free Style and Inter-war Art Deco and Late Twentieth Century International in Apperly, Irving and Reynolds, *Identifying Australian Architecture: Styles and terms from 1788 to the present*, 1989.



*This Building on the corner of Belmore Road and Waratah Avenue is perhaps the best example of inter-war Art Deco Architecture to be found in Randwick Junction.*

## **Part 3**

# **SITE PLANNING AND BUILDING DESIGN**



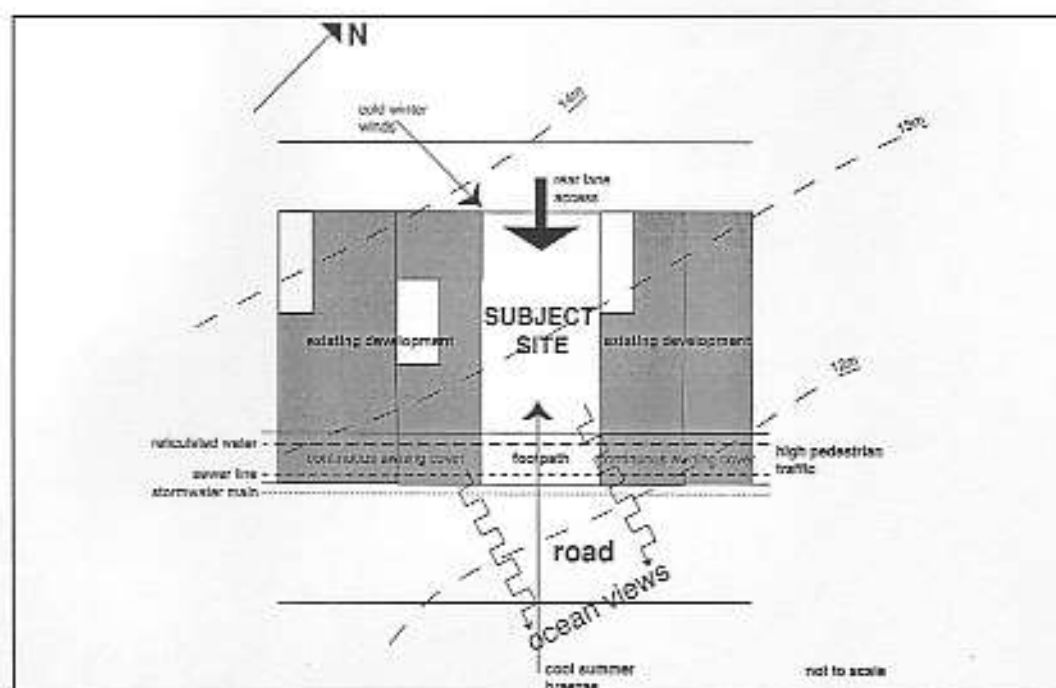
### 3.1 SITE ANALYSIS AND PLANNING

#### Objectives

- Achieve a coherent site layout that provides a pleasant, attractive, manageable, resource efficient and sustainable environment for living, commerce and study.
- Ensure that the local site conditions, positive attributes, constraints and opportunities of the site are taken into account in the design of new development.
- Ensure that new development responds to the topography of the site and its neighbouring areas.
- Ensure that the relationship of new development to adjoining development is considered in the design process and that negative impacts are minimised.

#### Explanation

Site analysis and planning is an essential component in the design of development proposals. It must take into account the existing environmental conditions such as vegetation, topography and drainage lines, view corridors, and microclimate. Site analysis enables the designer to work from a sound knowledge base when preparing site plans. Proper site planning will ensure that the positive attributes of an area will be enhanced in the design of a development proposal. Randwick Junction is located along a ridgeline, meaning that many sites will experience a fall to adjoining residential areas. The sloping terrain may be used to advantage by split-level and stepping developments facilitating the use of underground carparks or storage / servicing areas. However, care needs to be exercised to ensure that adverse impacts due to changes in ground level and the apparent height of new development on the uphill or downhill sides of neighbouring buildings is minimised.



*A sample diagram showing typical elements requiring consideration for an infill site in a commercial area. Note that the high degree of site cover on adjacent sites limits options for building orientation and siting and that the availability of rear lane access allows an active interface with the street.*

The Randwick Development Assessment (DA) Guide provides details and requirements for site analysis in applications to Council for development consent. Reference should be made to the DA Guide for detailed requirements for the preparation of site analysis diagrams.

## LEP Controls

### *Clause 2- Aims of the Local Environmental Plan*

Contains general controls safeguarding environmental amenity from impacts of new development.

### *Clause 43- Protection of heritage items, heritage conservation areas and relics*

Defines work requiring consent with respect to heritage items and heritage conservation areas. It also sets out matters for consideration when determining impact on heritage significance.

### *Clause 46- Development in the vicinity of heritage items, heritage conservation areas and potential archeological sites*

Establishes need for consideration of impacts on heritage significance for development of land in the vicinity of heritage items.

### *Clause 47- Conservation Incentives*

Contains incentives to encourage the retention of heritage items.

### *Clause 48-Conservation Plans*

Allows Council to defer granting of consent until a conservation plan has been prepared and considered if Council is concerned about heritage impacts.

### *Schedule 3*

Identifies buildings and places listed as heritage items.

### *LEP Maps*

Identifies the heritage conservation areas

### Site Layout and Building Design:

- Relates to the dimensions and shape of the site;
- Integrates with the surrounding area through consideration of the streetscape and landscape design components (latter where applicable);
- Takes into account the slope of the land and the desirability of minimising earthworks and retaining walls associated with building construction, enabling the development to achieve a balance between cut and fill, so as to minimise the transportation of fill to and from the site;
- Retains significant streetscape features such as stone fencing and retaining walls;
- Provides clear and legible pedestrian and cycle links with the street and open space networks;
- Takes into account, retains and integrates any item or feature of identified conservation or heritage value; Minimises the impacts on the amenity of neighbouring sites in terms of privacy, overshadowing, noise and views;
- Maximises microclimatic opportunities for solar access and ventilating breezes;
- Enhances personal safety and minimises potential for both crime and fear of crime;
- Provides for adequate servicing and parking arrangements for both occupants and customers / visitors, which is designed to reinforce the dominance of pedestrian activity within the streetscape.

A site analysis plan must be submitted with each development application involving new work (excluding minor works and additions) in accordance the requirements set out in the Randwick Development Application Guide.

## DGP Controls

## 3.2 DENSITY, HEIGHT AND

## SETBACK REQUIREMENTS

### Objectives

- Ensure that the height and massing of new development is consistent with the dominant heights and massing in the locality;
- Promote site densities of a level that are compatible and in scale with existing development through the use of floor space ratios, building height limits and building line setbacks; and
- Control the relationship of new development to the street and adjoining development.

### Explanation

The floor space ratio (FSR) is the primary control of building bulk and the density of development.

The building height and building line are the major factors determining the built form and character of a locality. The majority of existing development within Randwick Junction is built to the street alignment, with some notable exceptions.

Building heights controls are used to protect views of important landmarks and to preserve the overall consistency of building heights within Randwick Junction.

**LEP Controls***Clause 32- Floor Space Ratios*

The maximum Floor Space Ratios are 2:1 in the General Business 3A Zone and 0.9:1 in the Residential 2C zone, respectively. A maximum floor space ratio of 0.5:1 applies for development for residential purposes in the Special Uses Zone.

*Clause 33- Building Heights*

12 metres in the 3A General Business Zone in Randwick Junction

12 metres in the 2C Residential C zone

*Clause 47- Conservation Incentives*

Provides flexibility in relation to the range of permissible land uses. Allows Council to consider excluding the gross floor area of a heritage item for the purposes of determining the floor space ratio, landscaping and parking requirements.

*Clause 49- Definitions*

"floor space ratio" means the ratio of the total gross floor area of all buildings (existing and / or proposed) to the site area.

"gross floor area" means the sum of the areas of a building where the area of each level is taken to be the area within the outer face of the external enclosing walls, excluding:

- a) columns, fin walls, shading devices, awnings, balconies and any other elements, projections or works outside the general lines of the outer face of the external wall;
- b) lift towers, cooling towers, machinery and plant rooms, and air-conditioning ducts;
- c) associated car parking and any internal vehicular or pedestrian access to that parking; and
- d) space for the loading and unloading of goods.

"site area" in relation to development means the area of land to which an application for consent to carry out development relates, but does not include any part of that land on which the development is not permitted by or under this plan or any other environmental planning instrument.

**DCP Controls**

- New development should be built to the street alignment and to the side boundaries of the allotment. Where adjoining and nearby development is set back from the street, new development should be consistent with the setbacks of adjoining development or the dominant setbacks along the street.
- Where buildings are set back from the front boundary, fences are to be used to reinforce the street alignment and provide a strong visual transition point between public and private space.

### 3.3 FUNCTIONAL DESIGN REQUIREMENTS

#### 3.3.1 Pedestrian and Disabled Access

##### Objectives

- Ensure that appropriate levels of access to new developments are available to all sectors of the community; and
- Maintain a pedestrian focus within the streetscape environment.

##### Explanation

Randwick Junction is a centre that has a high degree of convenience for pedestrian users, owing to its compact, village-style form and the relatively flat grades over most of the commercial area. It also has a strong physical connection to the adjacent residential areas, which contributes to the easy pedestrian access to the centre.

New development should contribute to the overall accessibility of the centre and reinforce the roles of Alison Road, Avoca Street and (especially) Belmore Road as major pedestrian thoroughfares. Pedestrian / vehicular conflict needs to be minimised and car parks and vehicular servicing areas should be located away from pedestrian thoroughfares wherever possible.

It is also important to consider the access and mobility needs of disadvantaged sectors of the community. The *Disability Discrimination Act, 1992*, has consequences for Local Government and developers alike.

Council encourages building owners and developers to assess their liability under the *Disability Discrimination Act, 1992*. Australian Standard 1428: Access and Mobility, Parts 1-4, provides the most comprehensive guide for disabled access in Australia. Compliance with its requirements may be included as a condition to any consent granted.

##### LEP Controls

None Applicable

##### DCP Controls

- New development must address requirements for people with a disability, with particular reference to the provisions of AS 1428: Access and Mobility, Parts 1-4.
- Ground level commercial floorspace should incorporate "at grade" threshold access at the street frontage.
- Doorways in commercial areas are to be wide enough to provide access for people in wheelchairs or for parents with prams.
- Continuous accessible paths of travel (as defined in AS1428.1- Access & Mobility) are to be provided in new commercial developments in Randwick Junction and should include the use of non-slip floor and ramp finishes.



### 3.3.2 Vehicular Access

#### Objectives

- Provide adequate arrangements for vehicular access and the servicing requirements of commercial and residential land uses.
- Ensure that vehicular servicing areas do not visually intrude upon the public domain or obstruct thoroughfares.

#### Explanation

Vehicular access and servicing within Randwick Junction needs to operate in an efficient and effective manner to minimise the intrusion of such activities upon the pedestrian domain. Council intends that the Randwick Junction develop and maintain a character which maximises the enjoyment of the public realm and reduces the dominance of the motor vehicle. General controls for vehicular access and servicing are contained within Randwick DCP- Parking.

Kerb-side delivery of goods and the removal of waste within a commercial centre is incompatible with people-orientated activities such as outdoor dining and pedestrian circulation. Council's intention is for deliveries and waste collection to be separated from areas where pedestrian activity is concentrated wherever possible. Rear servicing areas in mixed-use developments should be able to cater for both residential and commercial servicing requirements.

#### LEP Controls

None Applicable

#### DCP Controls

- All vehicular access points and servicing areas are to be provided in accordance with the requirements of Randwick DCP- Parking.
- Vehicular access points are not to be located on the frontages of Alison Road, Avoca Street or Belmore Road unless other feasible alternatives are not available.
- Loading and unloading facilities are not to be located on the frontages of Alison Road, Avoca Street and Belmore Road, unless other feasible alternatives are not available.



### 3.3.3 Carparking

#### Objectives

- Provide appropriate levels of carparking for new development.
- Ensure that vehicular carparking areas do not intrude visually upon the public domain.
- Ensure that new car parks are designed in a manner that enhances the safety of the public, semi-public and private domains.

#### Explanation

The availability of appropriate levels of carparking that operates in an efficient manner within a commercial centre is one of the key determinants of the successful functioning of that centre. Inadequate, poorly located and poorly designed car parks can detract significantly from the amenity, safety and functionality of a commercial centre.

New development that provides insufficient carparking for its traffic-generating capacity can (in the absence of suitable public transport) lead to a reduction in the through traffic capacity of the streets and in the increased incidence of overflow parking in adjacent residential areas.

Poorly located and designed car parks can lead to the creation of spaces that are dangerous in times of low demand and can result in inconvenient linkages to desired destinations. In this regard, the location and design of carparking areas is integral to the design of a successful development.

#### LEP Controls

None Applicable

#### DCP Controls

- Carparking is to be provided at the rates required by Randwick DCP- Parking.
- On-site carparking is to be provided either at ground level or as basement carparking. The provision of above-ground carparking that is visible from the main pedestrian thoroughfares is undesirable and will not be supported where other feasible alternatives exist.
- The location of carpark ventilation grilles on the frontages of Alison Road, Avoca Street and Belmore Road is not permitted. The location of car park ventilation grilles on other street elevations is not recommended unless other feasible alternatives are not available.
- Carparking areas shall be adequately screened to minimise the visual impact on the proposed development adjoining properties and the locality.
- Private carparking areas should be well lit and secure and the entrance to the building interior is to be clearly visible from all spaces.

### 3.3.4 Solar Access and Energy Efficiency

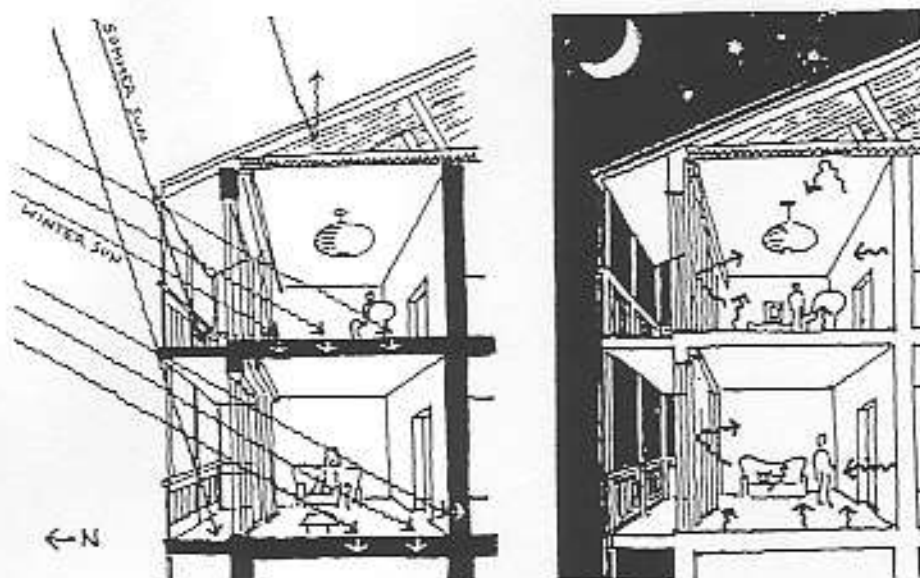
#### Objectives

- Orientate buildings to achieve the maximum solar access wherever possible and maximise the opportunities for use of natural light and ventilation throughout new development.
- Optimise energy usage by minimising the building's dependence on grid-supplied electricity through the use of alternative energy sources wherever feasible.
- Ensure that the long-term sustainability of materials used in construction is evaluated and that the use of non-renewable resources is discouraged.

#### Explanation

As part of its continuing commitment to Ecologically Sustainable Development (ESD), Council promotes the use of energy efficient design in new buildings. This lessens the overall energy consumption in Randwick City as well as contributing to wider regional energy efficiency goals. In order to achieve these objectives, the design of new buildings should give consideration to the following issues:-

- Solar Access;
- Thermal Mass of building materials;
- Water Heating Systems;
- Appliance Selection and Siting;
- Material Selection and Construction (use of renewable and recycled materials);
- Access to Natural Light within habitable rooms; and
- Use of Natural Ventilation.



*The benefits of orientating living spaces towards north. In summer the interior walls are shaded. In winter, sun enters and warms the internal bulk and concrete walls and floors, which release the warmth in the evening. Curtains and insulation help retain heat.*

It is recognised, however, that the existing pattern of development and the need to preserve the streetscape form (buildings occupying the entire street frontage) limits the opportunities for orientating buildings for maximum solar access. Therefore new development will need rely on use of internal configurations to ensure solar access to activity areas where these constraints exist. Many of the materials used in period buildings have a high thermal mass (due to extensive use of masonry) and the traditionally high ceilings and double-hung windows allow for natural light to penetrate deeply and contribute to natural ventilation. By following the general form and materials in period buildings in new development, significant gains towards energy efficiency can also be achieved.

#### LEP Controls

None Applicable

#### DCP Controls

- Commercial buildings are to be designed so that activity areas are sited and orientated to achieve at least four hours of sunlight between 9:00 am and 3:00 pm on June 21.
- The windows of the main activity rooms and living areas of both commercial and residential development should be shaded to minimise penetration of direct summer sunlight. Balconies may be used in situations where the use of hoods or other shade structures is inappropriate.
- Living area windows in buildings with a residential component are to be orientated and sized to achieve at least four hours of sunlight between 9:00 am and 3:00 pm on June 21.
- Buildings with a residential component are to be designed and sited so as to maximise the living areas facing north and minimise any overshadowing of the living areas of adjoining buildings.

### 3.3.5 Privacy and View Sharing

#### LEP Controls

None Applicable

#### Objectives

- Provide adequate and appropriate measures to ensure the privacy of residents / users of dwellings or premises above ground level.
- Provide for an appropriate outlook for each dwelling or premises above ground level.

#### Explanation

In order for new development to successfully integrate into the existing urban environment, care should be given towards minimising the opportunities for overlooking between developments, particularly development with a residential component. Attention should be given to window and balcony design and placement to avoid reductions in privacy to new occupants and existing neighbours.

It is considered important to the well being of residents in a mixed-use environment that they have a view that extends beyond their immediate surrounds. This contributes to the sense of place experienced within a locality and helps residents to feel that they are part of their surrounds. It also contributes to the casual surveillance of the public domain, which in turn reinforces perceptions of safety within the local community.

#### DCP Controls

- Windows and balconies of main living areas are to be located so as to avoid overlooking of similar windows in adjoining dwellings.
- Where there is a lateral separation of less than 10m between windows in adjacent buildings, offset alignments, splay windows or other appropriate treatments are to be used to reduce privacy effects.
- New development should not intrude on the important views and view corridors identified within Section 2.1 of this document.

### 3.3.6 Sound Insulation

#### Objectives

- Provide adequate sound insulation between individual dwellings or premises both within a building and between and from adjacent buildings.
- Provide adequate sound insulation from external noise sources such as vehicular traffic and air-conditioning plant equipment for new dwellings or premises.

#### Explanation

Acoustic privacy is one of the most important considerations in successful development, whether it be for commercial or residential use (or any combination thereof). New development should be designed to achieve appropriate levels of acoustic buffering between dwelling units and non-residential uses. Attention should also be given to acoustic attenuation between dwellings both within the building and between adjacent buildings.

Alison Road, Avoca Street and Belmore Road is a Sub-Arterial Road, are all subject to relatively high volumes of vehicular traffic and susceptible to subsequent noise problems which should be addressed in the design of new development.

#### LEP Controls

None Applicable

#### DCP Controls

- In locations or situations where noise from external sources such as heavily trafficked roads and commercial plant equipment is identified as being a potential problem for residential or office development, Council may require the submission of a report from a qualified acoustic consultant.
- The acoustic rating of common walls, floors and ceilings should minimise the possibility of sound transmission between dwellings / premises within a building.
- The internal layouts of dwellings and / or premises within a building should aim to separate the main living areas from communal areas (e.g. corridors, foyers etc.). Sleeping areas should also be separated from communal areas to minimise sleep disturbance.
- Plumbing pipes, heating units, air conditioning units and plant equipment should not be located adjacent to the main living and sleeping areas within a dwelling or adjacent to the main workspace within an office.
- Design and construction of new development (including refurbishment of existing development) should separate noise sensitive areas and activities from sources of undesirable of excessive noise.
- New development should be designed to achieve acceptable background noise levels for residential and commercial uses. Reference should be made to *Australian Standard 2107-1987 Acoustics- Recommended Design Sound Levels and Reverberation Times for Building Interiors*.



### 3.3.7 Safety and Security

#### Objectives

- Provide for appropriate levels of security within new developments.
- Ensure that new developments contribute to the overall safety of the public domain in Randwick Junction.

#### Explanation

Council intends that Randwick Junction be maintained as a "people friendly" centre, which is an inviting destination with a perception of being safe within the community. New development should be designed to contribute to the casual surveillance of the public domain through the location of windows, doors and other openings. Opportunities for activities to be carried out where they cannot readily be observed from the street or from within buildings should be avoided.

Where proposed development incorporates a residential component, or proposes the use of rear servicing / carparking areas (particularly without direct street / laneway frontage), additional considerations of matters of safety and security may be warranted.

#### LEP Controls

None Applicable

#### DCP Controls

- In mixed-use developments, clear distinction between or physical separation of the entrances to public, semi-public and private areas should be provided.
- Building facades should not use recessed doorways and windows at ground level with depths that could potentially conceal persons from casual surveillance from the street.
- Foyers should be designed at a human scale, doorways should be flush with the corridor walls (not recessed) and clear sight lines to alternative exits should be maintained.
- Corridor lengths and the number of dwellings / premises accessing each corridor should be minimised.
- Corridors should be designed so that they have access to natural light and ventilation.
- Buildings that contain multi-unit dwellings shall provide a security system that allows for quick and easy resident access and that minimises the time that residents and guests spend waiting in the public domain.
- Buildings should always address the street and excessive facade modulation that results in the creation of shadowed alcoves on street frontages is to be avoided. Similarly, large expanses of blank walls.



### 3.3.8 Private and Communal Recreation Areas

#### Objectives

- Provide adequate private and communal recreation areas for new residential development in Randwick Junction.
- Ensure that each area of private or communal recreation area is usable for the purpose for which it is intended.
- Ensure that the enjoyment of private or communal recreation areas does not impact on adjoining uses in an undue manner.

#### Explanation

Existing development within Randwick Junction contains a high proportion of mixed-use development with a residential component. There is a likelihood that this pattern of usage will be continued in new development. This creates a demand for both private and communal recreation areas within new development. It is therefore desirable that new residential development incorporate a balcony for each dwelling, which is readily accessible from the main living areas of that dwelling.

The provision of communal open space is not required for new residential development within this DCP area. However, opportunities for the provision of rooftop or internal communal recreation areas should be investigated for larger developments.

#### LEP Controls

None Applicable

#### DCP Controls

- For buildings with a residential component, it is necessary to provide a private recreation area for each dwelling in the form of a balcony.
- Balconies shall have a minimum area of 10 square metres and a minimum dimension of 2 metres.
- Balconies should be recessed into the facade so that they do not dominate the appearance of the building facade. Refer to Section 3.4 *Building Character* for further guidance.
- For buildings containing a large number of dwelling units it may be desirable to provide an area of rooftop communal recreation area. In such instances, parapets are to be of sufficient height to allow for the safe usage of the rooftop area and to prevent overlooking of any adjacent areas of private recreation areas.
- Noise impacts from the use of rooftop areas on the building occupants and neighbours are to be minimised. Where a potential significant noise impact exists, Council may require the submission of a report from a qualified acoustic consultant.

*Nb: enclosed recreation facilities such as indoor swimming pools, gyms and the like are included within calculations of the gross floor area of the development for the purposes of determining the FSR.*

### 3.4 BUILDING CHARACTER

#### Objectives

- Ensure that the built form of new development responds appropriately to the existing character of Randwick Junction.
- Ensure that new development is consistent with the distinctive character, aesthetic qualities and heritage significance of the precinct.

#### Explanation

The distinctive character of Randwick Junction is largely determined by the existing built form of the centre. Change within this area is envisaged as being incremental in nature and it is not intended for the centre to become dominated by new development with a modern character. New development is instead expected to conform to the parameters established within the existing character of Randwick Junction.

The form of new a building contains many design elements that contribute to its appearance, function and impact that on the surrounding area, such as:

- the parapet and / or cornice lines;
- balconies;
- fenestration;
- materials and colours;
- awnings; and
- facades / shopfronts.

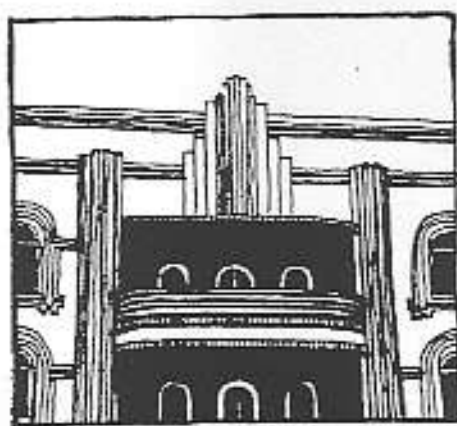
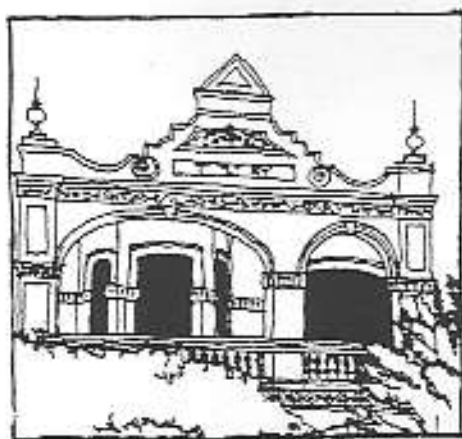
These design elements, and their treatment determine the relative success of a development in terms of:

- appearance;
- relationship with adjoining and nearby development;
- the contribution it makes to the character of the townscape and the locality; and
- the internal relationships within the site, including the perceived levels of comfort and satisfaction experienced by the occupants.

## LEP Controls

None applicable

*The illustrations above show different*



*parapet and balcony treatments in Randwick Junction. The architectural detail of new development should be derived from existing development.*

## DCP Controls

*Parapet/skyline*

- New development in Belmore Road shall include a parapet on the street frontage which obscures views of the roof.
- New development must include design elements at the parapet which are compatible with the architectural detail of existing development adjoining the site and in the locality. It is important to avoid mimicking or copying the detailing of particular periods. Instead an appropriate and respectful contemporary response is sought.
- Council may consider, on merit, the use of parapet design features which extend above the 12m height limit, where this is a significant character element on adjoining buildings or on prominent corner sites.

*Note: Any proposal exceeding the statutory height limit must be accompanied by a SEPP 1 objection.*

*Balconies*

- Recessed balconies which modulate the facade should be incorporated in the design of new development. The use of cantilevered balconies is to be avoided on new buildings.
- Balconies should be designed to protect the visual amenity of the future occupants and the locality. Required balustrades (as defined by the Building Code of Australia) should therefore be either solid or have the appearance of being solid when viewed from the exterior of the building.
- Balconies should be sized and arranged in such a manner that strong horizontal lines do not dominate the facade of the development.



*Row of early 20th Century shops, circa 1920, at 10-126 Belmore Road. Note the parapet details, vertical emphasis and fenestration placement.*

#### *Materials and Colours*

- The materials and the way in which they are used in the finishes of the building should reflect the materials used in adjoining and nearby development. Council will not accept radical departures from themes in Randwick Junction.
- Acceptable materials include face brick (including polychromatic and decorative elements) and rendered masonry (including roughcast stucco). Use of other materials such as glazed tiles as feature elements may be acceptable in some situations. The use of precast concrete is to be avoided.
- Acceptable roof materials include painted corrugated iron, Marseilles tiles and slate. On commercial buildings the roof form should be concealed behind the parapet when viewed from the street.
- Colours should enhance the locality and will preferably be drawn from the colour palette detailed in *Knox and Tanner, Urban Design Study of Randwick and "The Spot" Shopping Centres, 1994, Volumes One and Two, Building Inventory*. The general colour schemes are listed in Appendix 2 of this DCP.

For Advice on appropriate colour selection for particular buildings, consult with Councils Department of Environmental Planning and City Development.

- Where face brickwork is used the predominant brick colourings shall be reds, browns and blue/black manganese to match the brickwork in existing development.

#### *Shopfronts*

- Shopfronts must be designed to reinforce the character of the locality and to ensure continuity of experience at the street level. The form, scale and proportion of shopfront elements should respond to the nature of the existing nearby development.
- The use of aluminium fittings on shopfronts is discouraged. Wherever possible wood should be used for window and door frames.
- The use of roller shutters is discouraged as they form an unwelcoming facade element outside of business hours and contribute to fear of crime in the locality. The use of security measures that relate to the architectural character of the area, such as expanding metal grilles are a preferred solution.

**Part 4**  
**OUTDOOR ADVERTISING AND RESTAURANT**  
**SEATING**



## 4.1 OUTDOOR ADVERTISING

### Objectives

Outdoor advertising should:

- complement the character of Randwick Junction;
- complement the character of Randwick's environmental heritage
- enhance the character of a building, site and/or area.
- convey the advertiser's message while conforming to the character of the precinct

### Explanation

"How signs are designed and where they are placed affects the character of the environment. The grouping of signs can influence the effectiveness of individual signs and can either enhance or spoil other environmental qualities." (NSW Department of Urban Affairs and Planning, *Outdoor Advertising: An Urban Design Based Approach*)

Council encourages provision for appropriate outdoor advertising at the earliest stages of the design process. The scale of signs should relate the proposed development, adjoining and nearby buildings, and the street width. The general controls for outdoor advertising in business and residential zones, including heritage conservation areas are contained in Randwick DCP- Outdoor Advertising.

The use of above awning signage is prohibited in Randwick Junction. Council encourages the removal of existing above awning signage in Randwick Junction and its replacement with new signs in appropriate locations such as on drop blinds and awning fascias consistent with this DCP and the recommendations of the Knox and Tanner Urban Design Study, Vol. 1.

For further information reference should be made to Council's Outdoor Advertising Development Control Plan.

### LEP Controls

None applicable

### DCP Controls

- All signage should be erected in accordance with the provisions of Randwick DCP- Outdoor Advertising.
- Signs should reinforce the architectural character of individual buildings and the valued qualities of the townscape. Special attention must therefore be given to the design and placement of advertising, particularly in highly visible locations, including corner sites.
- The replacement and/or removal of nonconforming and obsolete signs is encouraged and will be considered in assessing development proposals, including alterations and additions to existing buildings.
- The installation of "drop blind" type signs suspended from awnings are encouraged.

## 4.2 OUTDOOR RESTAURANT SEATING

### Objectives

- to encourage a lively pedestrian environment in Randwick Junction
- to provide opportunities for outdoor dining while protecting the rights of public access

### Explanation

Council encourages the establishment of cafes and restaurants within Randwick Junction as these uses contribute to the vitality and character of the wider community. The provision of footpath dining areas can enliven the streetscape of the area and adds to the richness and diversity of experience available within Randwick Junction.

However, the introduction of footpath dining areas may create conflicts over usage of the road reserve area and the introduction of unsympathetic items of street furniture into the streetscape may disrupt its heritage integrity. A series of controls have been implemented, through both this DCP and Randwick DCP 20- Restaurants on Public Road Footways; Airspace Above Roads and Public Land.

A person wishing to establish an outdoor dining area on a footway in Randwick Junction must obtain consent from Council in the form of a Development Approval (DA), under the Environmental Planning and Assessment Act.

### Information required for a submission

A DA for outdoor restaurant seating should include a site plan that contains at least the following information:

- the dimensions and shape of the area to be used;
- any existing toilet facilities within the restaurant;
- the number and location of tables and chairs in both the existing restaurant and the outdoor seating area;
- the design material and location of any fence, planter boxes or paving intended to define the area;
- the location of all utility structures adjacent to the proposed site including but not limited to power poles, trees street furniture, parking sign posts;
- the proximity of pedestrian or vehicular entrances serving adjoining buildings.

A written submission is also required stating:

- the hours of operation of the existing restaurant and the proposed footway restaurant; and
- whether the existing restaurant is currently licensed to serve alcohol and whether it is proposed to serve alcohol at the proposed footway restaurant.

A DA must be accompanied by the consent of all land owners. The Council is the owner of all public roads within the City of Randwick other than freeways or Crown roads for which the consent of the RTA or the Minister for Roads is required respectively.

**LEP Controls**

None applicable

**DCP Controls**

- All areas used for outdoor restaurant seating are to comply with the provisions of Randwick DCP 20- Restaurants on Public Road Footways; Airspace above Roads and Public Land.
- The use of a footway for a restaurant is only permissible if it is operated in conjunction with a restaurant adjacent to the footway.
- The minimum unencumbered residual footway width is 2.5m

- Approval is required under the Roads Act to use a footway for the purpose of dining (Section 125). A combined application (DA/Roads Act) may be lodged with Council.
- Council will require all planter boxes, fences and paving to be consistent with the Mainstreet program for Randwick Junction.
- Development consents for footway restaurants will be limited to a period which will not exceed seven years.
- Contributions for amenities and services may be payable to council as required by Council's Section 94 Plan.



*Example of successful integration of outdoor restaurant seating into the pedestrian environment.*

## **APPENDICES**

## A1 HERITAGE AND STREETSCAPE SCHEDULES

### A1.1 Schedule of Heritage Items Randwick LEP 1998; Permanent Conservation Orders (Heritage Act 1977) and Listings on the Register of The National Estate.

Address	Randwick LEP Heritage Schedule #	Permanent Conservation Order #	Register of the National Estate Database #
191 Alison Road ("Seabird")	14		
193 Alison Road ("Glanmire")	15		
200 Alison Road	16		
Cnr Alison Road & Avoca Street ("Jubilee Fountain")	56		
110-116 Avoca Street ("Hetta Building")	66		
115-139 Avoca Street	67		
124 Avoca Street ("Randwick Post Office")	68		001769
126-138 Avoca Street	69		
146-150 Avoca Street ("Clovelly, Illfracombe & Torquay")	70		
147 Avoca Street ("Coach & Horses Hotel")	71		
156-158 Avoca Street	73		
160-162 Avoca Street ("Somerset & Glastonbury")	74		
194 Avoca Street	76		
200 Avoca Street ("Former Star & Garter Inn" or "Hannan's Butchery")	79		001773
Cnr Avoca Street & Belmore Road ("Statue of Captain James Cook, RN")	95		001750
35-43 Belmore Road	96		
48-60 Belmore Road	97		
70-82 Belmore Road	98		
84-90 Belmore Road ("Koowari Flats")	99		
119 Belmore Road ("Cooks Lodge")	100		
120-126 Belmore Road	101		
127-129 Belmore Road	102		
128 Belmore Road ("Sandgate")	103	67	
17-19 Clara Street	150		



## 1.2 Schedule of Buildings of Architectural Merit / Positive Streetscape Contribution (Source Knox & Tanner Pty Ltd "Urban Design Study of Randwick Junction and 'The Spot' Shopping Centres, Vols. 1&2")

Legend	Architectural Significance		Streetscape Contribution	
	E	Extreme	CI	Critically Important
	H	High	I	Important
	M	Moderate	C	Contributes
L	Low	D	Detracts	
Address	Randwick LEP Heritage Schedule #	Knox & Tanner Architectural Significance Rating	Knox & Tanner Streetscape Contribution Rating	
165 Alison Road		H	I	
167-171 Alison Road		H	CI	
173 Alison Road		H	I	
177-179 Alison Road		H	CI	
191 Alison Road ("Seabird")	14	E	CI	
193 Alison Road ("Glanmire")	15	E	CI	
195-197 Alison Road		H	I	
200 Alison Road	16	E	CI	
201-203 Alison Road		H	CI	
202-204 Alison Road		M	I	
110-116 Avoca Street ("Hetta Building")	66	E	CI	
115 Avoca Street	67	M	C	
117-121 Avoca Street		H	I	
120 Avoca Street		ML	C	
124 Avoca Street ("Randwick Post Office")	68	E	CI	
123-127 Avoca Street	67	M	C	
129-131 Avoca Street	67	H	I	
130-138 Avoca Street	69	H	I	
133-135 Avoca Street	67	H	I	
137 Avoca Street	67	II	CI	
139-145 Avoca Street	67	H	I	
140-142 Avoca Street		M	C	
144 Avoca Street		M	C	
146-150 Avoca Street ("Clovelly, Ilfracombe & Torquay")	70	E	CI	
147 Avoca Street ("Coach & Horses Hotel")	71	II	CI	

151 Avoca Street		M	I
152-154 Avoca Street		H	I
156-158 Avoca Street	73	E	CI
160-162 Avoca Street ("Somerset & Glastonbury")	74	H	I
194 Avoca Street	76	H	I
200 Avoca Street ("Former Star & Garter Inn" or "Hannan's Butchery")	79	E	CI
2-10 Belmore Road		H	I
7 Belmore Road		H	I
9 Belmore Road		M	C
11 Belmore Road		H	I
16-18 Belmore Road		H	I
20 Belmore Road		H	I
23 Belmore Road		M	C
25 Belmore Road		M	C
29-31 Belmore Road		M	C
33 Belmore Road		M	C
34-38 Belmore Road		M	C
35-43 Belmore Road	96	E	CI
40-42 Belmore Road		M	C
48-60 Belmore Road	97	E	CI
49-51 Belmore Road		M	C
62-68 Belmore Road		H	I
70-82 Belmore Road	98	M	I
84-90 Belmore Road ("Koorvari Flats")	99	H	I
96-104 Belmore Road		M/L	C
112 Belmore Road		M	C
119 Belmore Road ("Cooks Lodge")	100	M	I
120-126 Belmore Road	101	M	C
123 Belmore Road		M	I
127-129 Belmore Road	102	H	CI
128 Belmore Road ("Sandgate")	103	E	CI
135 Belmore Road		M	C

## A2 RECOMMENDED COLOUR SCHEMES

### COLOUR SCHEMES BY PERIOD

These colour schemes are general suggestions for buildings from various periods of architecture represented within Randwick Junction. They have been sourced from "Urban Design Study of Randwick and "The Spot" Shopping Centres- Volume Two - Building Inventory", Knox & Tanner, 1994. All colours are from Pascol Paints colour range, unless otherwise specified. For advice on appropriate colours for particular buildings, contact our HeritagePlanner.

#### Group 1 Colonial, Early Victorian, Mid Victorian, Late Victorian

BUILDING ELEMENT	RECOMMENDED COLOUR(S)		
	<i>Scheme A</i>	<i>Scheme B</i>	<i>Scheme C</i>
Brick Walls	Not painted		Unpainted
Stucco Walls; generally	Vellum	Rose Pink Flat	Drab
Stucco Entablatures and Parapets	Cream		
Entablature Frieze and Parapet		Sienna	
Entablature Architrave and Cornice; Parapet Coping; Frieze Brackets; Parapet Piers		Indian Red	
Cornice; Eaves Fascia and Gutters; Label Moulds			Dark Stone
Window Frames, including Transom Lights	Terra Verte		
Window Sills		Indian Red	Brown Pink
Window Frames		Indian Red	Dark Stone
Window Sashes	French Grey	Sienna	Dark Stone
Door Frames			Dark Stone
Doors	Venetian Red		Dark Stone
Decorative Iron Frieze and Brackets	Bronze Green		Brown Pink
Decorative Iron Balustrading	Bronze Green picked out with French Grey		Brown Pink
Verandah Fascias	Venetian Red	Ivory or Indian Red	Gaul Grey
Verandah Gutters	French Grey		
Moulding under Gutters	French Grey		
Verandah Roof Stripes	Terra Verte and Cream		
Verandah Ceiling	Eau-de-Nil	Sea Green or Pale Eau-de-Nil	Gaul Grey
Downpipes	As for adjacent surfaces		

**Group 2 Edwardian**

BUILDING ELEMENT	RECOMMENDED COLOUR(S)		
	<i>Scheme A</i>	<i>Scheme B</i>	<i>Scheme C</i>
Brickwork of Walls and Chimney	Unpainted	Unpainted	
Walling generally			Federation Cream
Parapet Copings and modelled detail			Simpson Sand
Stone Trim	Unpainted	Unpainted	
Window and Door Frames, including Mullions and Transoms	Light Stone	Manilla	
Window Frames			Avocado
Window Sashes	Biscuit	Manilla	Avocado or Federation Cream
Doors and Transom Lights	Dark Crimson	Eau-de-Nil	
Exposed Rafter Ends and Gable Barge Boards	Biscuit	Eau-de-Nil	
Gable Barge Moulding below edges of roof covering	Dark Crimson	Mid Green	
Roughcast Stucco, eg. in gable	Biscuit	Manilla	
Gable Cover Battens and embellishments	Dark Crimson	Eau-de-Nil	
Aprons below window sills	Light Stone	Manilla	
Eaves, Gutters and Downpipes	Dark Crimson	Same as surround walling, eg. brick shade, red brown	
Soffits eg. eaves	Biscuit	Manilla	
Awning Fascias			Norfolk Green
Awning Ceiling			Federation Cream or Avocado
Verandah Posts and Beams	Dark Crimson	Manilla	
Decorative Timber Valances	Biscuit	Manilla	
Cast Iron	Biscuit	Manilla	
Other Metalwork, eg. fences	Dark Crimson	Manilla	

**Group 3      1920's, Art Deco, 1930's/1940's**

BUILDING ELEMENT	RECOMMENDED COLOUR(S)		
	<i>Scheme A</i>	<i>Scheme B</i>	<i>Scheme C</i>
Brickwork	Unpainted		
Main Wall Areas		Dairy Cream	York Stone <sup>1</sup>
Cement Render Detailing	Sea Corn <sup>1</sup>	Dairy Cream	
Other Cement Render Accents	Cream*		
Embellishments, eg. date on facades			Bath Stone <sup>1</sup>
Roughcast Panels and Panels below windows (where applicable)			Bath Stone <sup>1</sup>
Door and / or Window Frames	Cream <sup>1</sup>	Carnival	
Door and / or Window Frames	Seawitch <sup>1</sup>		
Window Frames and Sashes			Bronze Olive <sup>1</sup>
Doors		Eau-de-Nil	
Eaves Gutters		Volcano	
Eaves Fascias		Carnival	
Fascia / Fascia Awning	Botanic <sup>1</sup>		Wildfire Red <sup>1</sup>
Awning Ceiling or other Soffits	Seawitch <sup>1</sup>		Confetti <sup>1</sup>
Front Fences		Dairy Cream	

<sup>1</sup> Dulux Colours



**Group 4**      **1950's, 1960's / 1970's, 1980's**

BUILDING ELEMENT	RECOMMENDED COLOUR(S)		
	<i>Scheme A</i>	<i>Scheme B</i>	<i>Scheme C</i>
Brickwork	Unpainted*		
Parapet	Pale Peach <sup>2</sup>		Golden Lime <sup>2</sup>
Parapet Panels			Mist Green <sup>2</sup>
Parapet Piers and Capping / Window Frames			Iron Bark
Parapet Edging	Brown Pink <sup>2</sup>		
Lettering on Parapet			Iron Bark
Signage Panels	Brown Pink <sup>2</sup>		
Awning Fascia		Military Blue	Iron Bark
Awning Ceiling	Morning Sky <sup>2</sup>	Iceland	Off White
Rendered Shopfront Piers		Woodland	
Rendered Parapet		Woodsmoke	
Rendered Walling			Golden Lime
Modelled Embellishment		Claypot	

\* If bricks intended to be seen as facework have been painted and it is impossible to remove the paint, apply a colour to simulate the colour of brickwork.

<sup>2</sup> Wattle Solagard Colours

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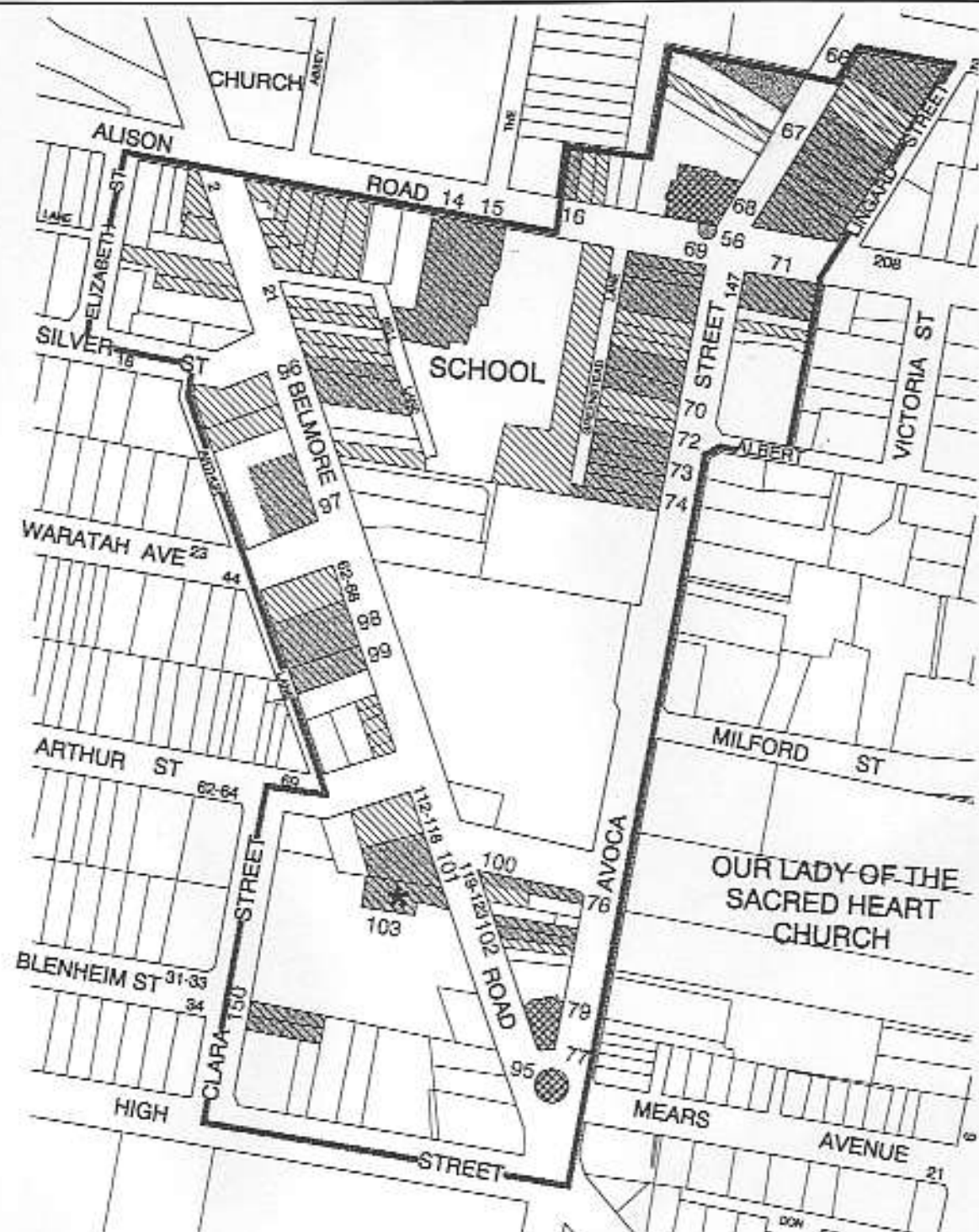
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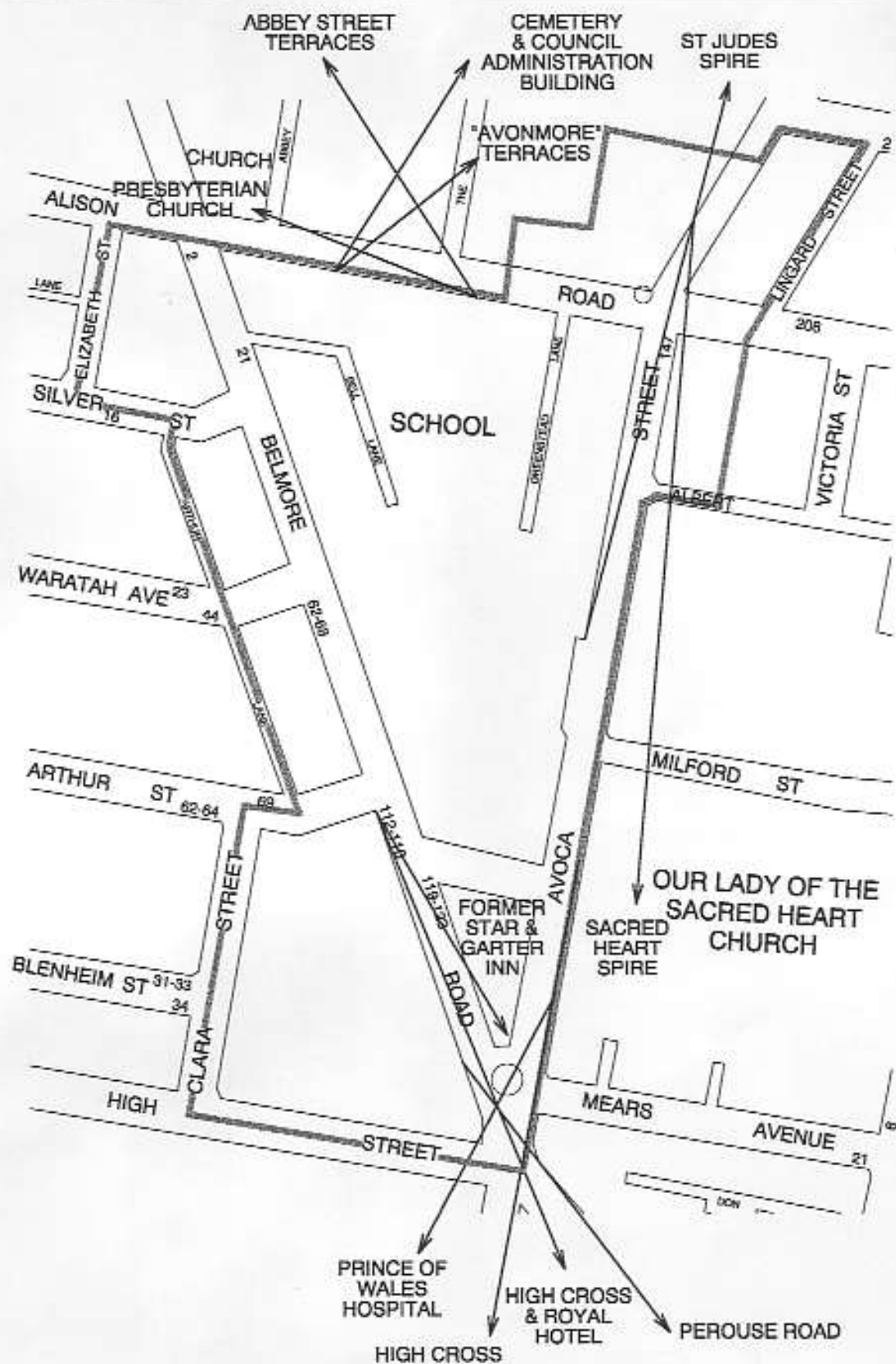
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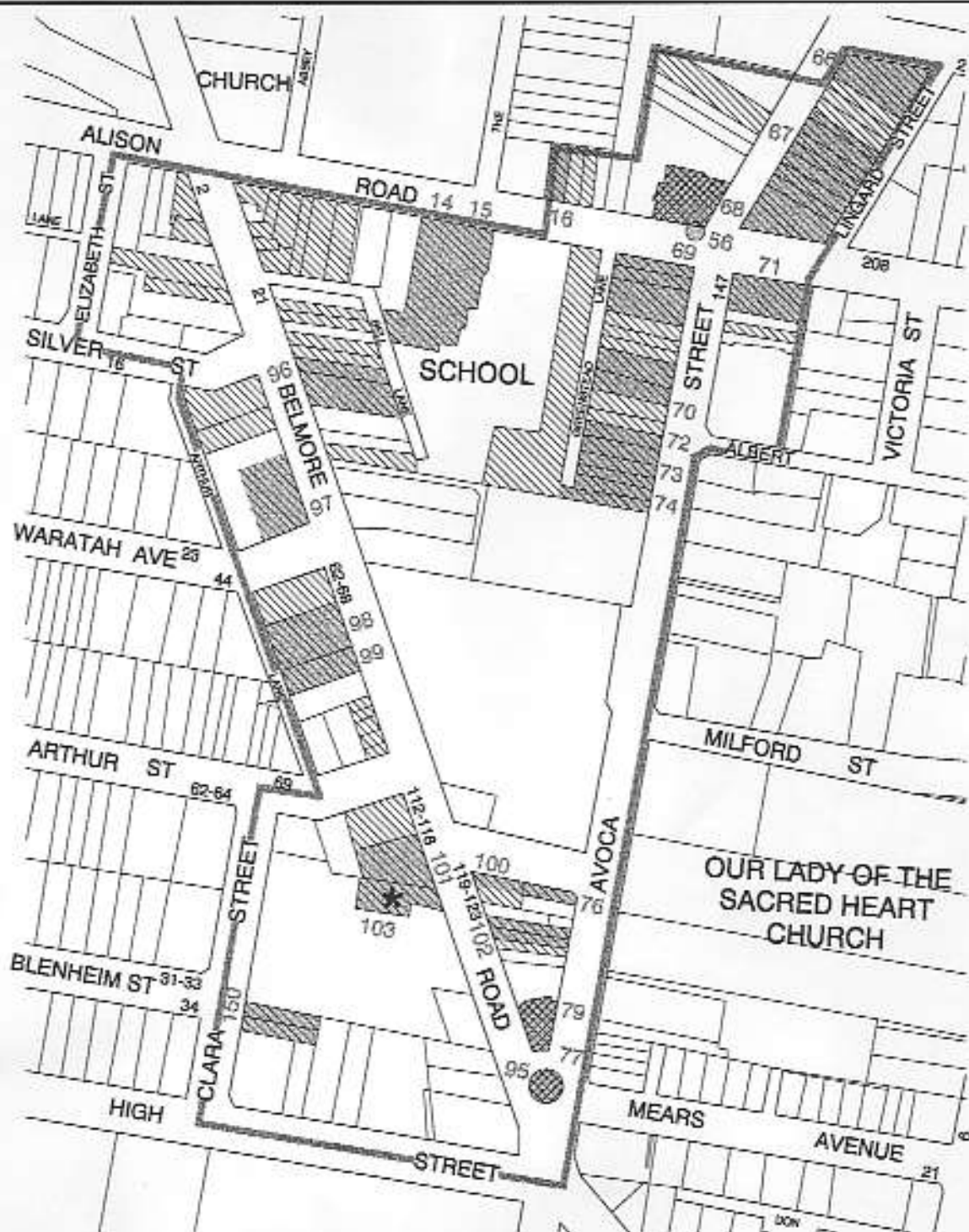
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-  HERITAGE CONSERVATION AREA
-  HERITAGE ITEM (RANDWICK LEP 1998)
-  BUILDINGS WHICH CONTRIBUTE TO STREETSCAPE CHARACTER (URBAN DESIGN STUDY OF RANDWICK & "THE SPOT" SHOPPING CENTRES KNOX & TANNER PTY LTD, 1994)

-  PERMANENT CONSERVATION ORDER (HERITAGE ACT)
-  REGISTER OF THE NATIONAL ESTATE (AUSTRALIAN HERITAGE COMMISSION)

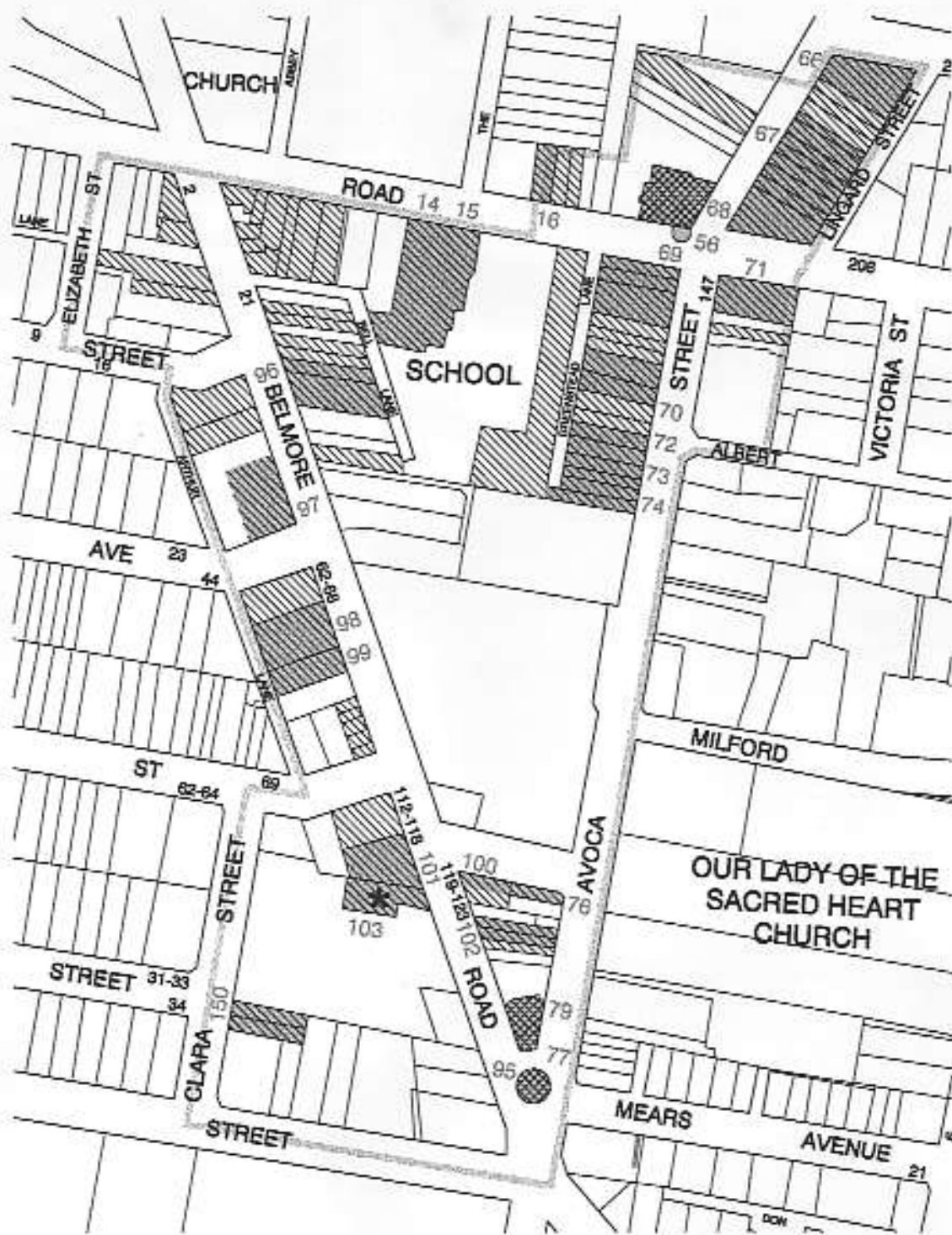




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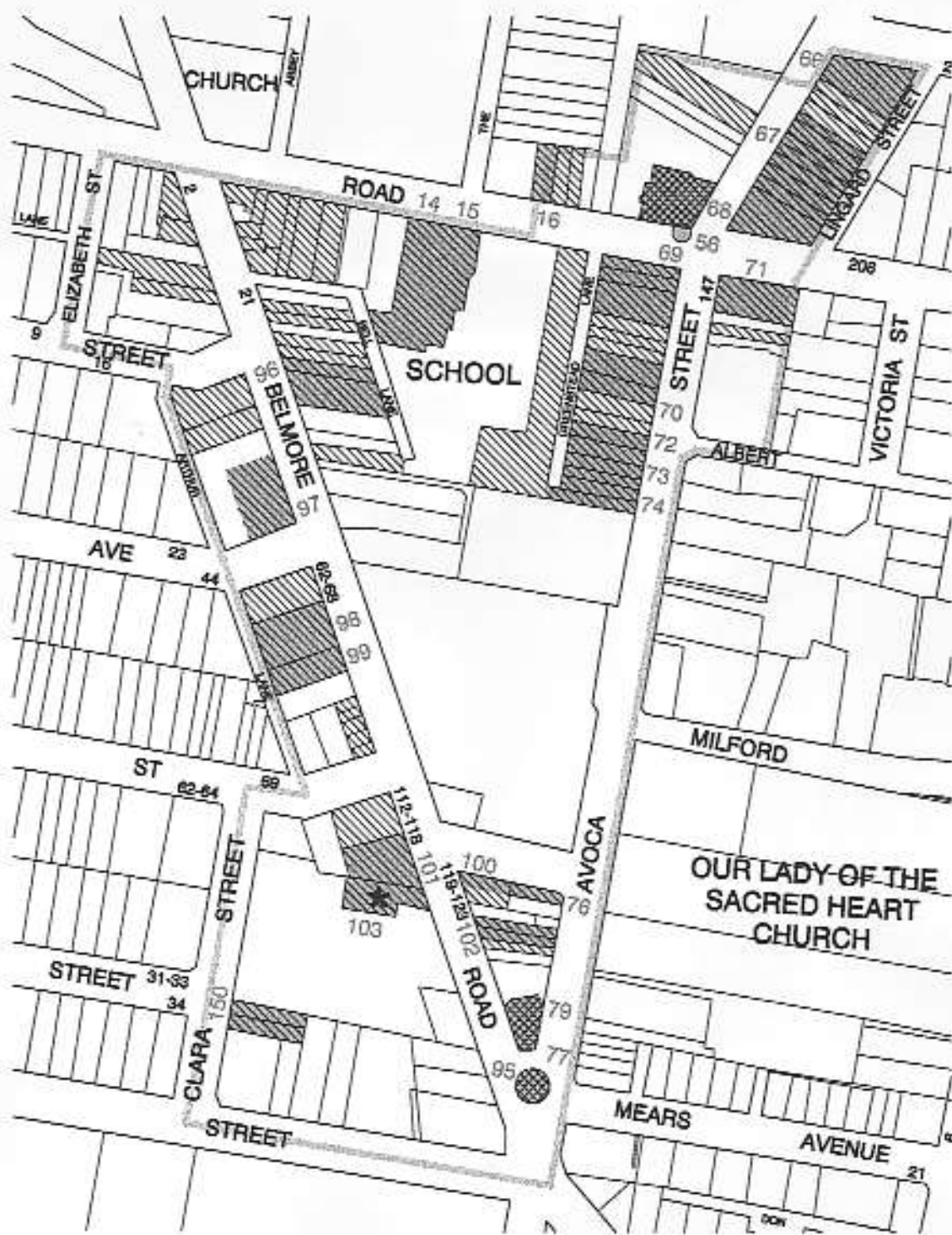
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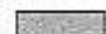

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

LOCALITY RANDWICK

DCP - RANDWICK JUNCTION

RANDWICK HERITAGE AND STREETSCAPE CONTROLS



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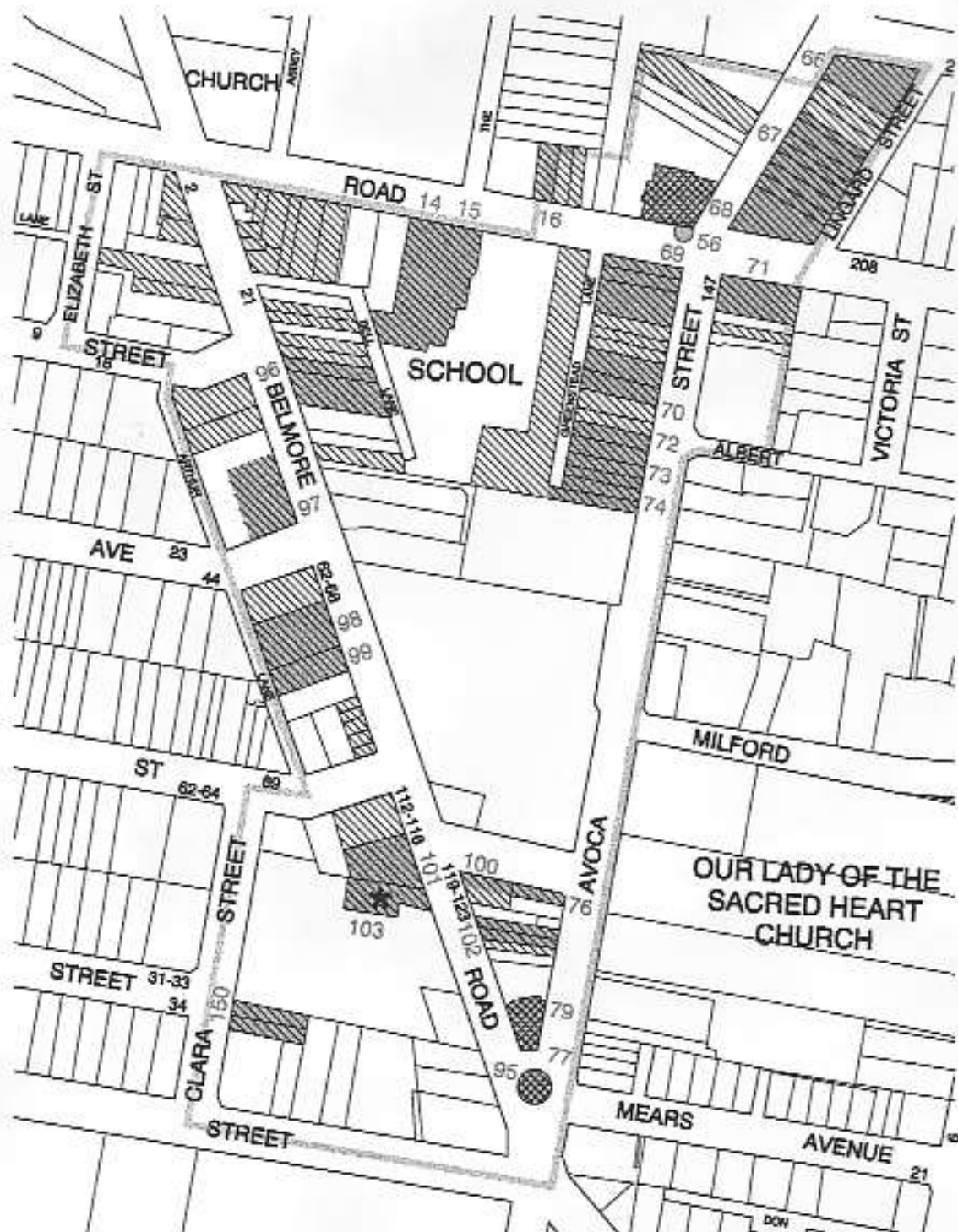
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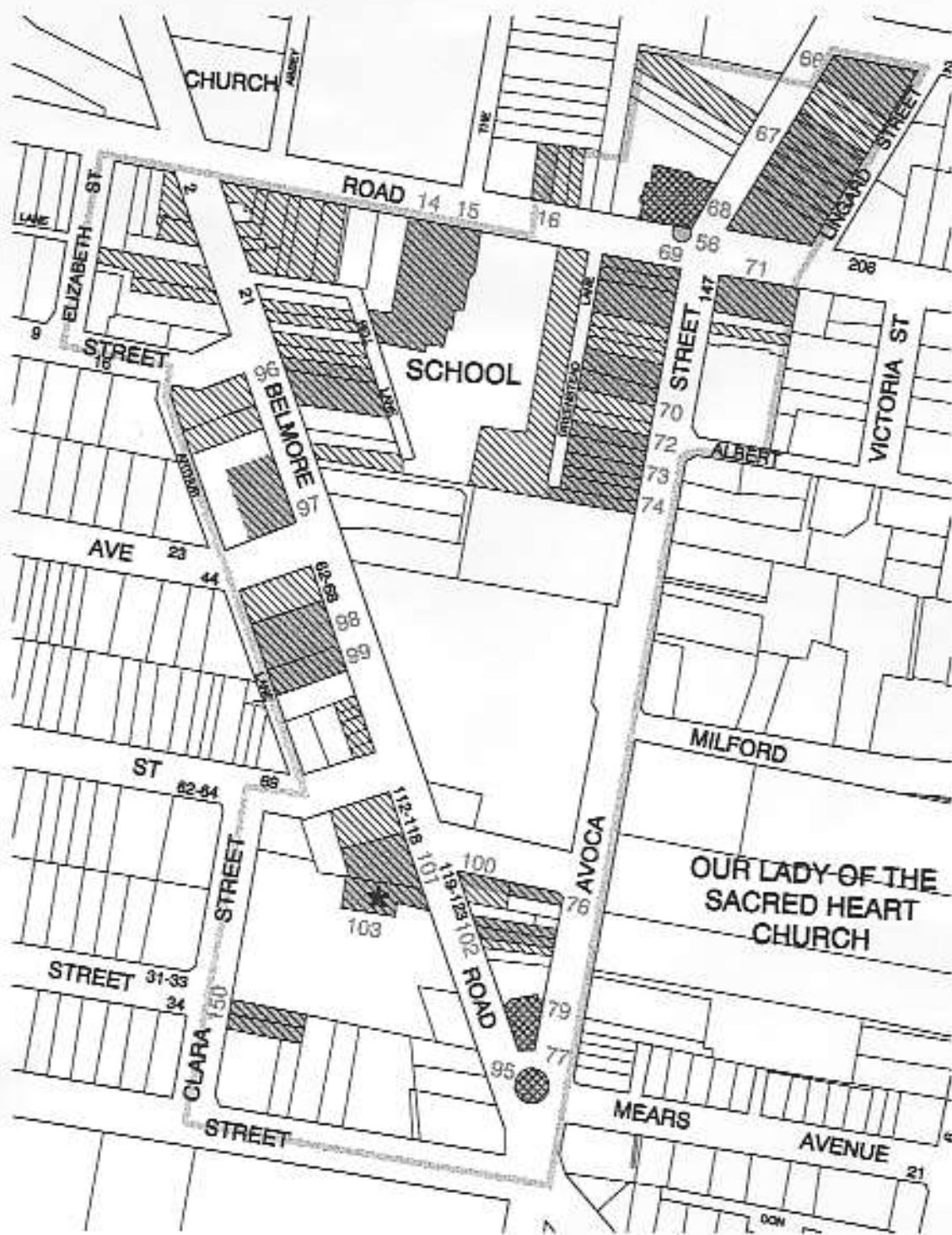
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

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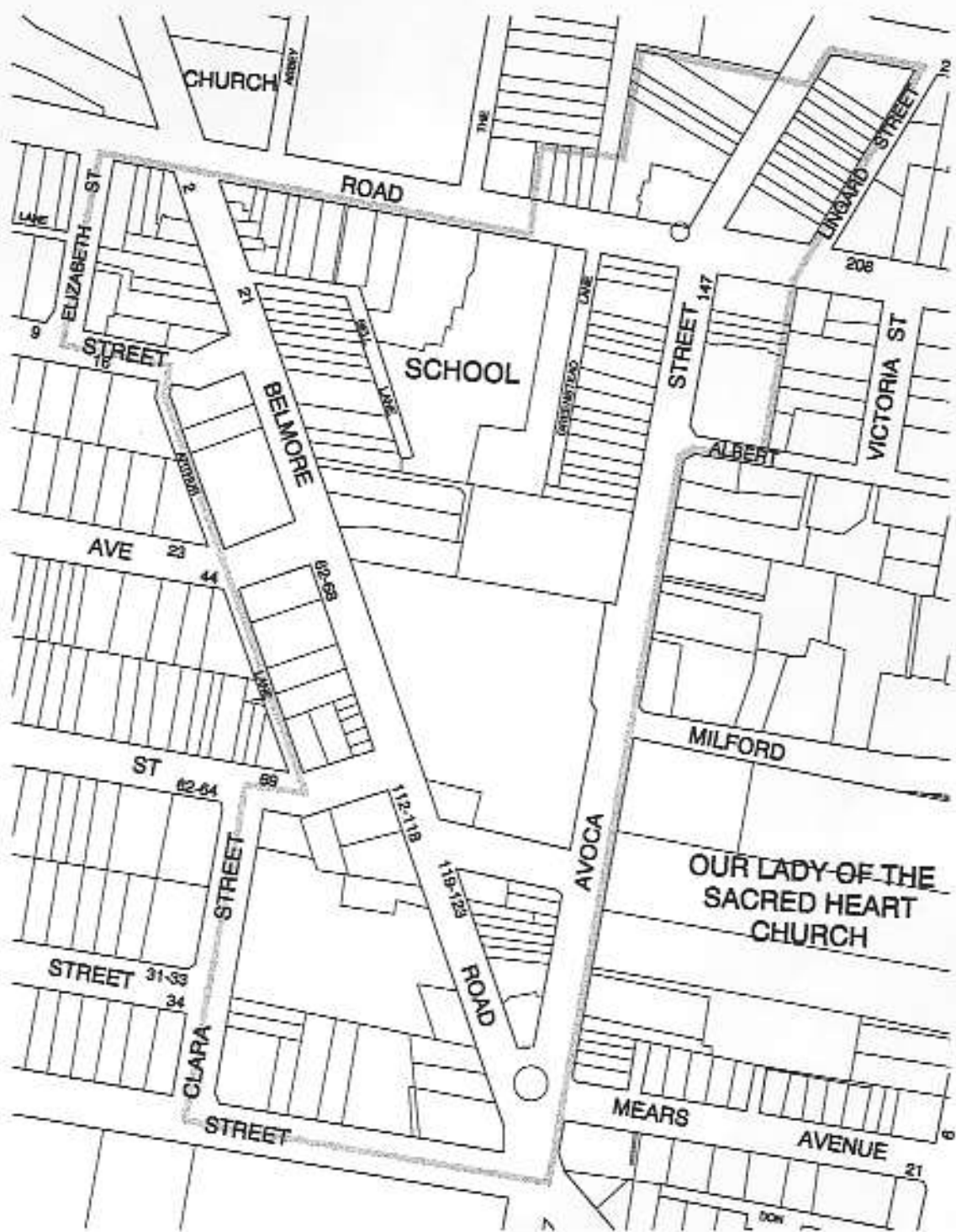
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## RANDWICK HERITAGE AND STREETSCAPE CONTROLS





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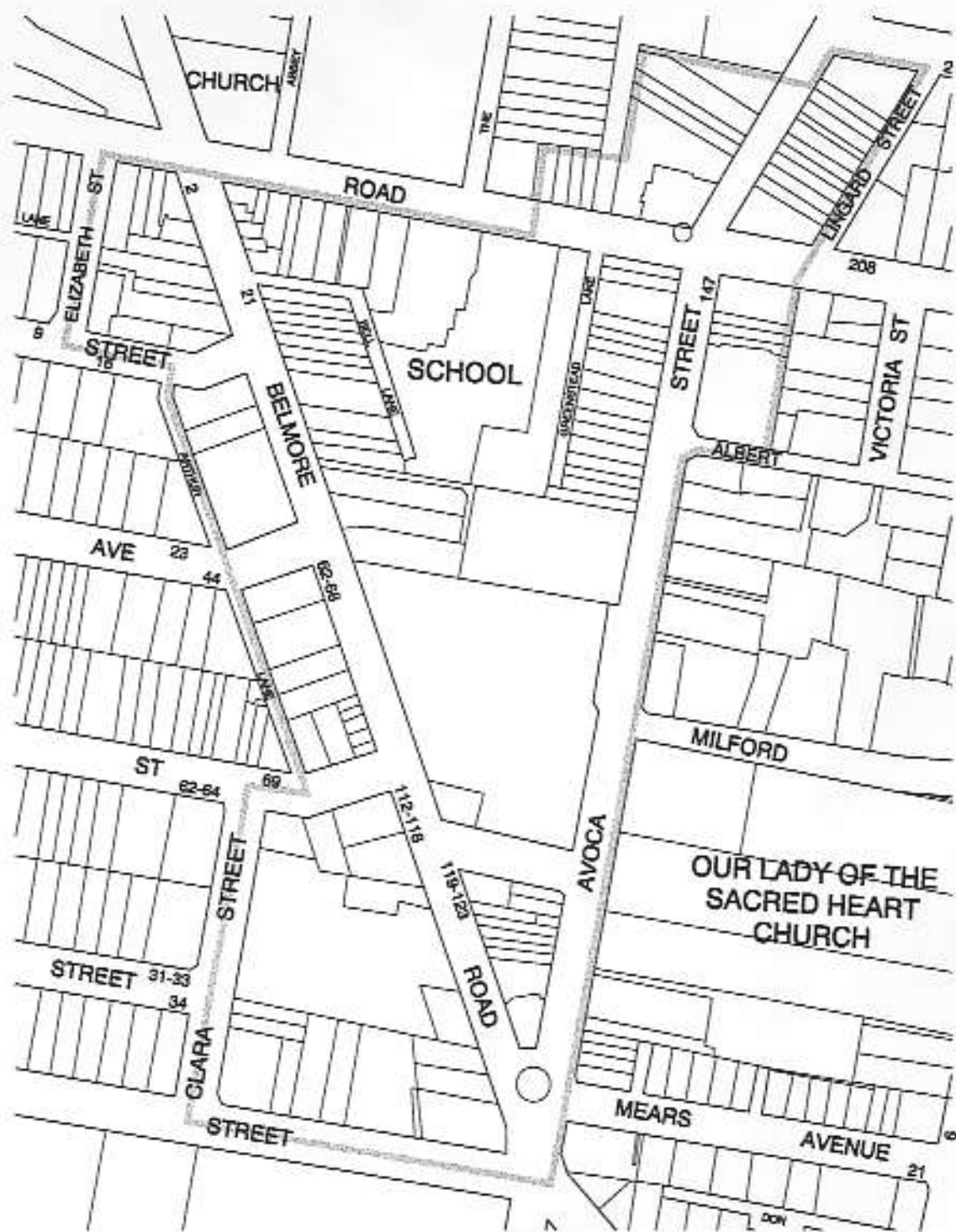
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HERITAGE CONSERVATION AREA  
 (Area to which this plan applies)

# RANDWICK DEVELOPEMENT CONTROL PLAN 1999

RANDWICK JUNCTION





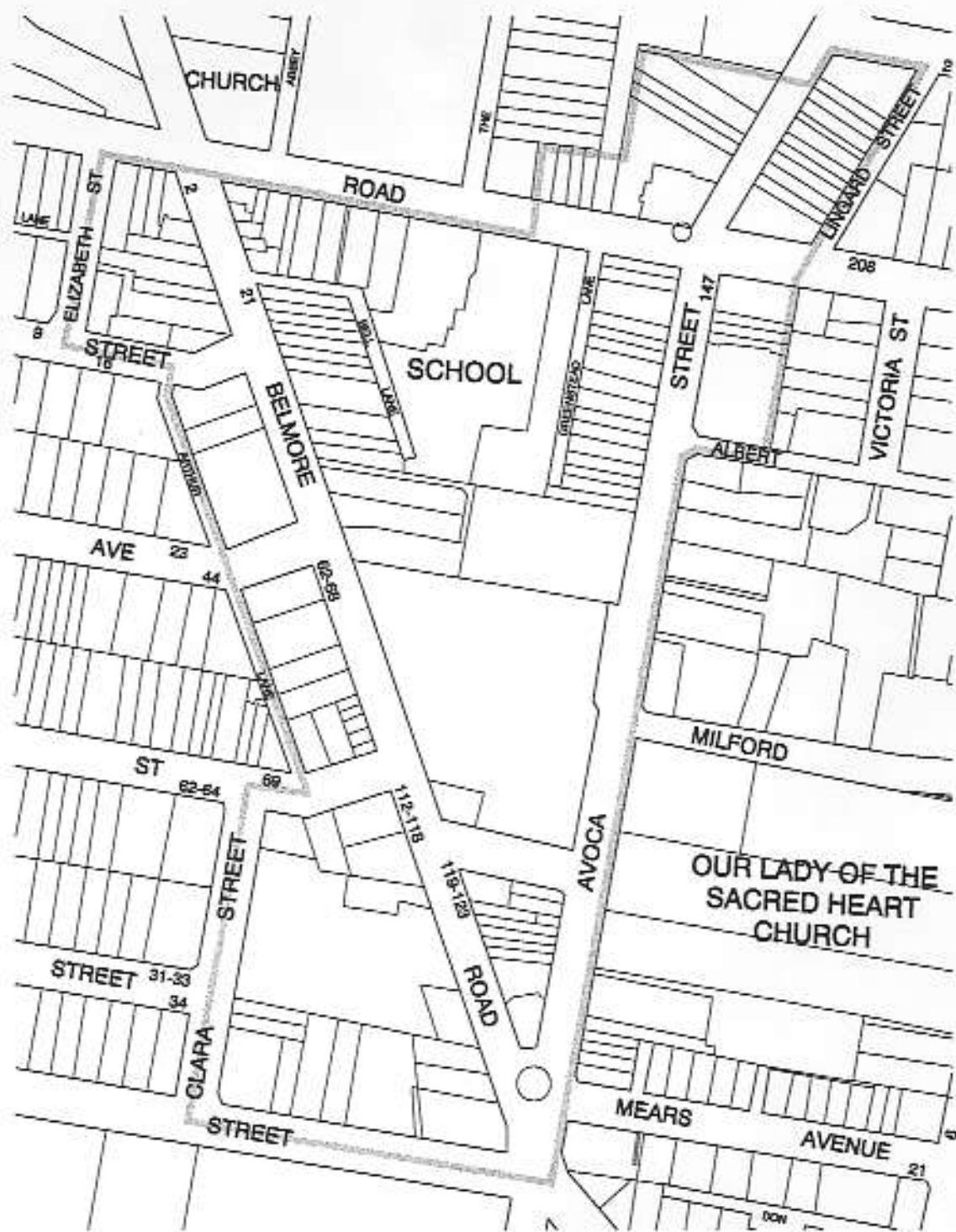
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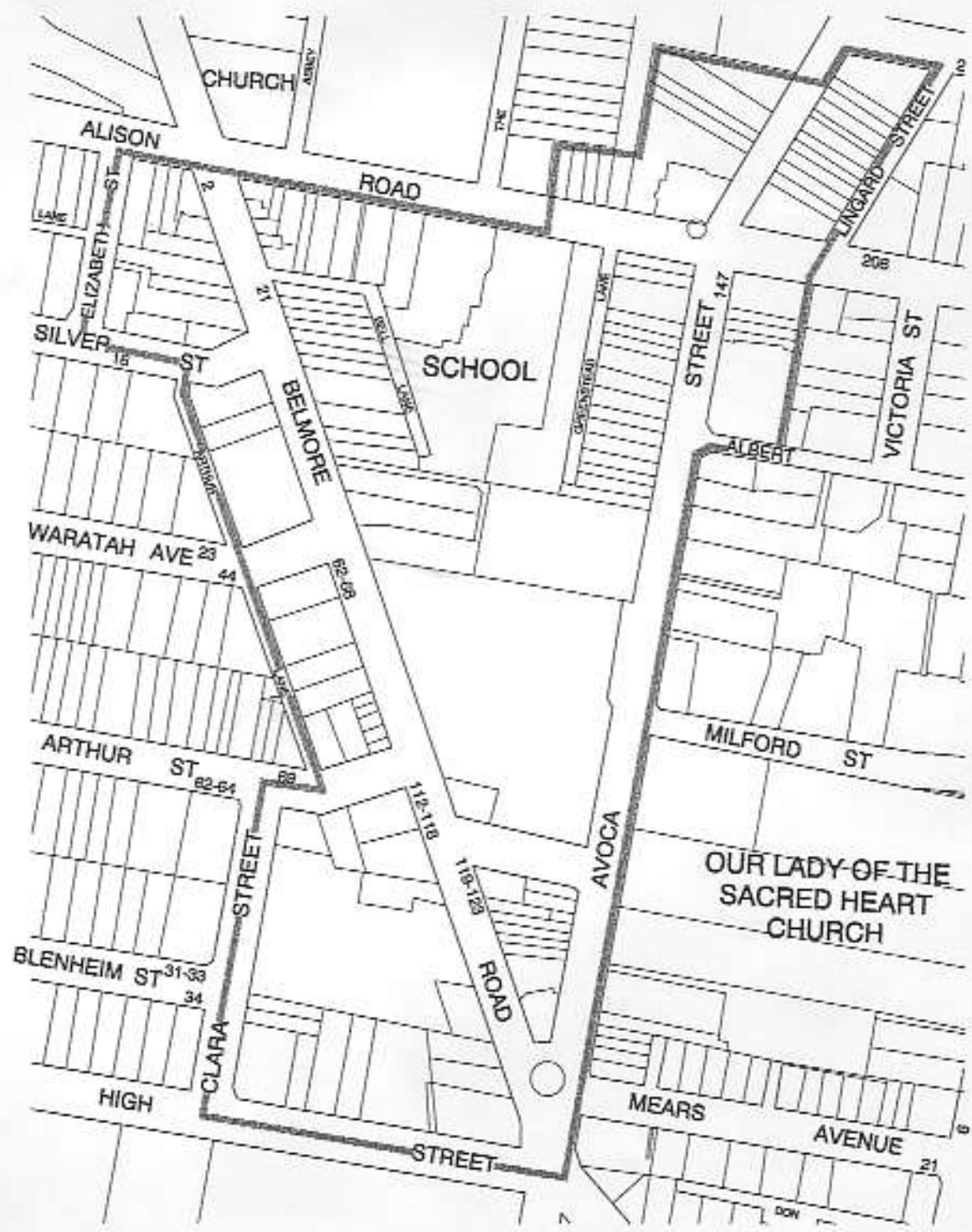
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